

Village of Homer Glen

159th Street Corridor Planning Study



October 28, 2015

DRAFT
FOR VILLAGE BOARD
CONSIDERATION

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Thank you to all the residents, business owners/operators, stakeholders, and agency representatives that participated in development of the **159th Street Corridor Planning Study**. The input provided by those who participated has been incorporated into the Plan's recommendations and will serve as a guide for future development along 159th Street.

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Chapter 1

Introduction & Context

The 159th Street Corridor Planning Study provides the Village of Homer Glen with a unique and valuable opportunity to plan for, and create, a high quality commercial corridor that serves both the residents, local businesses and visitors to the Village. 159th Street is an arterial street through the Village of Homer Glen with a full access interchange to Interstate 355. 159th Street is viewed as a very important commercial corridor. The Village of Homer Glen's 2005 Comprehensive Plan designated most of the property within the 159th Street Corridor as commercial. While some commercial development has taken place along the corridor, there remains a significant amount of undeveloped and available land with potential to accommodate the Village's vision for the corridor and help shape the future growth and development of the community.

Purpose of the 159th Street Corridor Planning Study

The 159th Street Corridor Planning Study is Homer Glen's official policy guide for physical improvement and development within the Corridor. It considers not only the immediate needs and concerns of the area and community, but also projects improvements and developments several years into the future.

The 159th Street Corridor Planning Study helps preserve and protect important existing features and resources, coordinate new growth and development, identify new infrastructure needs to foster new growth and development, and establish a strong and positive community image and identity for the Corridor.

The Study is "comprehensive" in both scope and coverage. It encompasses the use of land; the movement of vehicles and pedestrians; the protection of open spaces and environmental resources; and the provision of parks, trails, and other public facilities.

The 159th Street Corridor Planning Study establishes the "ground rules" for private improvement and development. It provides guidelines by which the Plan Commission and Village Board can review and evaluate private development proposals.

The 159th Street Corridor Planning Study will assist in the allocation of the Village's resources by identifying recommendations to be integrated into the Village's Capital Improvements Plan.

Project Overview

This 159th Street Corridor Planning Study focuses specifically on 159th Street within the set planning boundaries of the Village of Homer Glen. The Study serves as an addendum or overlay to the Comprehensive Land Use Plan, last amended in 2007, illustrating and specifying more specific recommendations for 159th Street.

The 159th Street Corridor Planning Study provides the necessary level of detail and planning to provide proper and effective guidance for the improvement of the Corridor within Homer Glen. Because 159th Street is within the jurisdiction and control of the Illinois Department of Transportation (IDOT), recommendations for the Corridor are realistic and achievable within the funding and control restrictions that exist within the Study Area, and reflect planned improvements for 159th Street. Key considerations of the Corridor Plan include:

- Identifying and prioritizing specific improvements that are needed to spur development along the Corridor
- Establishing desired and appropriate land uses and their locations along the Corridor
- Obtaining an understanding of the market conditions of the Corridor
- Specifying the types of businesses desired, appropriate and feasible for the Corridor
- Evaluating existing site design standards, landscaping requirements, lighting enhancements, and sign regulations
- Identifying specific recommendations to improve pedestrian/bicycle circulation along the Corridor

Corridor Study Planning Process

The 159th Street Corridor Planning Study planning process incorporated a six-step work program that entailed community outreach, analyzing existing land use and market conditions, developing preliminary plans, and preparing a final 159th Street Corridor Planning Study plan and policy recommendations. The process was designed to produce an 159th Street Corridor Planning Study plan that accommodates desirable new growth, development and redevelopment, while preserving and protecting open space and the Village's rural character. The plan also identifies and addresses the issues and concerns expressed from the various community outreach workshops, meetings, and questionnaires conducted throughout the study.

Organization of the Corridor Study

The Plan consists of eight (8) chapters presented in a concise, illustrative format to facilitate the clarity of recommended policies, and actions.

Chapter 1: Introduction & Context

This chapter provides a brief overview of the background and purpose for the Study, as well as a review of the planning process, existing conditions, and results of the community outreach activities.

Chapter 2: Market Conditions of the Corridor

This chapter presents an analysis of existing market conditions which was used to supplement land use and development recommendations of the 159th Street Corridor.

Chapter 3: Land Use & Development Plan

The Land Use & Development Plan provides policy direction for the future improvement, growth, and development within the Corridor. Site specific development concepts provide a guide for opportunity sites along the Corridor.

Chapter 4: Design & Development Guidelines

This chapter provides design objectives, guidelines, and recommendations for the aesthetic improvement of the 159th Street Corridor. The recommendations and guidelines are based on the desires expressed by members of the community throughout the planning process.

Chapter 5: Streetscape & Beautification Plan

The Streetscape & Beautification Plan provides recommendations for streetscape improvements including pedestrian and vehicular scale lighting, street trees, street furniture, way-finding signage, bus shelters, and continuous and interconnected pedestrian pathways.

Chapter 6: Access & Transportation Plan

The Access & Transportation Plan provides recommendations for improving vehicular, bicycle, pedestrian and public transit circulation and facilities.

Chapter 7: Utilities & Infrastructure Plan

This chapter identifies the future need for utilities and infrastructure and offers long-range recommendations for future facility locations and improvements to ensure that residents are adequately served by the Village and its service providers.

Chapter 8: Economic Development & Implementation Plan

The Economic Development & Implementation Plan sets forth specific actions required to carry out the recommendations of the Corridor Study. This section addresses various components of implementation actions including administrative actions, regulatory controls recommendations, and capital improvements.



Planning Context

The Village of Homer Glen is located in northeastern Will County primarily within Homer Township and partially within New Lenox Township, approximately 25 miles southwest of downtown Chicago. The Village is approximately 22 square miles in area and has an unofficial 2010 census number of 24,220. The Village of Homer Glen was incorporated in 2001 for the purpose of bringing local control to various issues impacting the quality of life, property values, and the local tax base.

Study Area

The Study Area for the 159th Street Corridor encompasses the Village's Planning Area, which includes areas outside of the Village's existing corporate limits, but within its planning jurisdiction and agreed upon limits with the City of Lockport, its neighboring community to the west. For the purpose of the Corridor Study, it is important to note Homer Glen's boundary agreement with Lockport as it pertains to 159th Street. The boundary runs along Gougar Road, north of the 159th Street Corridor, east along 159th Street, and continues south along Cedar Road, south of 159th Street.

The State of Illinois permits a municipality to plan for unincorporated areas 1.5 miles from their "municipal boundary", stating:

"Any Municipality may adopt [a] development plan applicable to contiguous land within 1.5 miles of municipality's corporate limits and not included in any other municipality." (Municipal Code 65 ILCS 5/1-1-1 through 11-152-4)

Earlier this year, the Village of Homer Glen hired Houseal Lavigne Associates, a professional consulting firm specializing in community planning, urban design and economic development, to work with the community to develop an economic development and planning corridor study that reflects the vision and character of Homer Glen.

The primary focus of the plan is to improve the economic vitality, appearance, image and accessibility of the 159th Street Corridor. This plan builds upon the Village's Comprehensive Plan prepared in 2005, and a 159th Street Corridor Plan prepared in 2010.

Although the 2005 Comprehensive Plan established desired land uses for the Corridor along with policies for future development, it considered the community as whole and its recommendations were more general. The 2010 159th Street Corridor Plan, prepared by engineering consultants HNTB, was prepared on a larger and more regional scale for six communities between Interstate 355 and Interstate 294, including Orland Park, Tinley Park, Homer Glen, Markham, Oak Forest and Orland Hills.

Corridor Setting

159th Street is the major arterial road of the region, running from Interstate 355 Veteran's Memorial Tollway west to Lockport and then connecting east to Orland Park. The surrounding areas around the Corridor remain largely undeveloped; for example, just before it meets the highway, 159th Street passes through several vacant parcels of land in Lockport. Yet, there has been significant commercial development to the north and east of the Corridor in Lemont and Orland Park in the form of big box retail, malls, and clustered shopping centers.

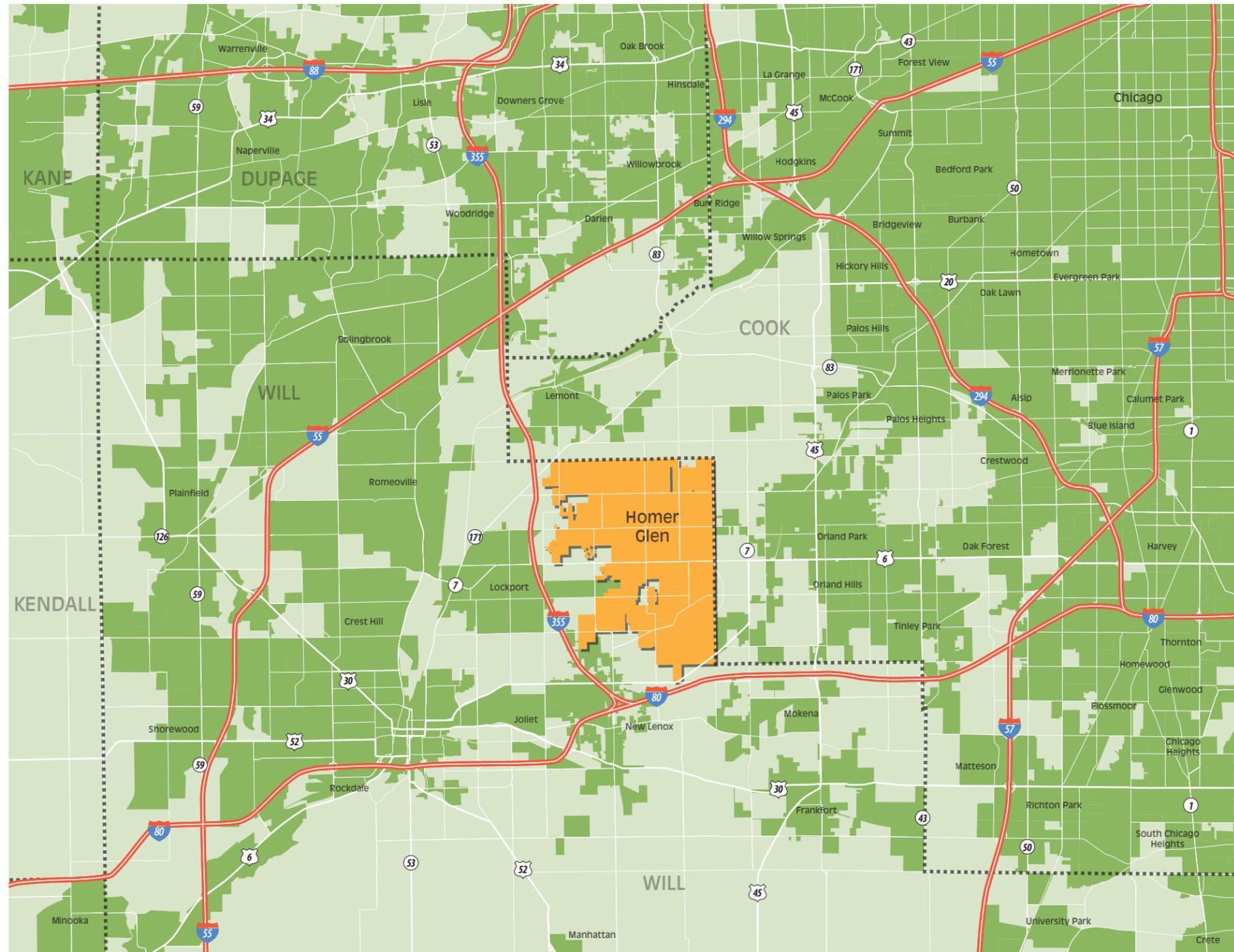
The 159th Street Corridor specifically is located in a 4.3 square mile study area in the center of the Village, bordered by Lemont to the north, Lockport to the west, New Lenox and Mokena to the south, and Orland Park to the east. It is also a short trip away from the Silver Cross Hospital and the Orland Square Mall. For the purposes of this study, the boundary of the Corridor begins at Gougar Road to the west and Will Cook Road to the east, excluding the area south of 159th Street from Gougar Road to South Cedar Road.

Interstate 355 Extension

The new Interstate 355 extension serves Will County to provide a regional connection that improves north-south mobility between Interstate 55 and Interstate 80. The extension is part of the Illinois Tollway System and connects Interstate 80 in New Lenox to Interstate 290 in Addison, meeting Interstate 55 in Bolingbrook and crossing Interstate 88 near Downers Grove. Construction of the project began in Spring 2006 at the site of the new Interstate 80 interchange at Cedar Road, and the toll road opened in November 2007, at a cost of \$730 million. The 12.5 mile, six-lane South Extension of the Veteran Memorial Tollway runs through 13 Municipalities/Townships in three counties including: Bolingbrook, Downers Grove Township, DuPage Township, Homer Glen, Homer Township, Lemont, Lemont Township, Lockport, Lockport Township, New Lenox, New Lenox Township, Romeoville and Woodridge.

The project has had a significant impact on reducing congestion and travel times for the area's commuters. The extension reduces travel times to suburban job centers by 20 percent while additionally alleviating traffic on local roads, which previously had been the only option for residents to travel to the regional road network. Before the extension's completion, residents would have to travel either west to Interstate 55 or east to Interstate 294 to access a north-south toll-road. Increased accessibility has also served as a catalyst for economic growth in Homer Glen as well as in the entire Chicago region. As one of the Village's primary access routes to Interstate 355, circulation for the 159th Street Corridor is integral to efficient access to the Interstate.

Regional Setting



Planning Context Map



Corridor Setting Map

Community Outreach

A variety of community outreach efforts were undertaken by the Consultant Team and Village Staff to involve property owners, businesses, Village residents, elected and appointed officials, and other stakeholders in the planning program. These outreach efforts provided the Steering Committee, staff, and the Consultant Team with important insight on issues, concerns and opinions from residents, community leaders, the business community, and other participants as they related to the 159th Street Corridor.

This section summarizes the many outreach activities undertaken and highlights some of the responses given by attendees. Outreach exercises included:

- Steering Committee Meeting and Workshop – January 29, 2013, at Homer Glen Village Hall
- Community Workshop, with residents and business owners – March 20, 2013 at the Homer Glen Village Hall
- Corridor Visioning Workshop, with residents and business owners – June 26, 2013 at the Homer Junior High School Cafeteria

Steering Committee Project Initiation Workshop

A project initiation workshop was conducted with the 159th Street Corridor Planning Study Steering Committee on Tuesday, January 29, 2013 at the Homer Glen Village Hall. The meeting included a review of the Corridor plan process, preliminary project schedule, and discussion of the overall direction and policy issues facing the 159th Street Corridor. The workshop included a questionnaire designed to gather initial input from the Steering Committee that helped frame some of the important issues addressed in the 159th Street Corridor Planning Study. The results of the questionnaire and related discussion are summarized below.

Overview

The steering committee workshop included a four-part questionnaire that focused on issues and concerns facing 159th Street. Steering Committee members were asked to identify and prioritize the most important issues facing the 159th Street Corridor, suggest desired actions to be undertaken, and list the strengths and assets of the Village of Homer Glen. The following includes a summary of the workshop as well as an inventory of responses to the workshop questionnaire.

Questions #1 & #2

Question #1 – Identify five (5) issues or concerns regarding the 159th Street Corridor in Homer Glen and Question #2 – Rank the three (3) most important issues discussed thus far.

Participants were first asked to list five concerns they had for the corridor. Following a discussion of the group's responses, participants were then asked to prioritize the collective list.

IDOT Road Improvement

The most important issues identified by the Steering Committee were the potential effects of IDOT's planned improvement of 159th Street, including the installation of a new center median. Highest among these were concerns about limiting access to the existing businesses along the Corridor, due to both new physical obstructions and the timing of the project.

Economic Benefit

Committee members were focused on increasing and maximizing the economic potential of 159th Street for the Village. They expressed a desire for the Study to enhance economic viability through recommendations that attract new businesses while retaining and supporting existing ones.

Image & Identity

An attractive appearance and a clear sense of place is an important part of drawing shoppers to visit 159th Street businesses and allowing new businesses to establish themselves. Committee members would like to see the Study outline recommendations and policies to improve the image and identity of the Corridor through things like streetscaping, signage, lighting, gateway features, building materials, etc.

Pedestrian Access & Safety

159th Street in Homer Glen is designed for cars, with minimal infrastructure for pedestrians or bicyclists. The Steering Committee ranked pedestrian access throughout the Corridor as an important subject for the Study to address. This includes improving the safety of pedestrians and bikers with changes to both the physical environment and the speed and pattern of traffic in the Corridor.

Other Issues

Other issues and concerns mentioned by the Steering Committee included:

- Annexation
- Communication with IDOT
- Extending sewer and water lines
- More efficient development regulations
- Overhead utilities
- Site cross access
- Stormwater management
- Updated zoning

Question #3

Identify three (3) specific projects or actions you would like to see undertaken regarding the 159th Street Corridor.

The Steering Committee was then asked to suggest projects that could address problems in the Corridor. Their ideas included:

- Develop a consistent gateway and image program
- Work with IDOT to alter existing improvement plans, mostly by eliminating the proposed median
- Create a set of design guidelines
- Meet and communicate with residents and business owners regularly
- Extend sewer, water, and other infrastructure to potential growth areas
- Strategically plan for growth and development
- Perform a detailed economic analysis
- Put utility lines underground
- Annex property westward to Interstate 355
- Review the current development ordinances
- Update the Comprehensive Plan
- Improve access to businesses
- Advise the Village Board about development realities
- Create a "downtown" destination



Question #4

What are the greatest assets, strengths, and advantages of Homer Glen with regard to the 159th Street Corridor?

The Steering Committee listed what they think are the positive features of the 159th Street Corridor and the Village of Homer Glen. They said:

- A great location with access to Interstate 355, nearby communities, and Cook County
- Available land for development
- The sense of community in Homer Glen
- The residents and demographics
- The existing business community
- Natural assets, including forest preserves and a growing trail network
- A willingness to grow and work with developers
- Planned IDOT improvements

Community Workshop

A community workshop was conducted for the 159th Street Corridor on Wednesday, March 20, 2013 at the Homer Glen Village Hall. The meeting included a review of the Corridor plan process, preliminary project schedule, and discussion of the overall direction and policy issues facing the 159th Street Corridor. The workshop included a questionnaire designed to gather initial input from the community that helped frame some of the important issues addressed in the 159th Street Corridor Planning Study. The results of the questionnaire and related discussion are summarized below.

Overview

The community workshop included a five-part questionnaire that focused on issues and concerns facing 159th Street. Community members were asked to identify and prioritize the most important issues facing the 159th Street Corridor, suggest desired actions to be undertaken, and list the specific uses/development they do, and do not want to see along the Corridor.

Residents identified a wide range of issues and concerns during the community workshop. These issues and concerns have been consolidated into the following categories:

- IDOT Road Improvements
- Economic Benefit
- Image and Identity
- Pedestrian Access and Safety

Question #1

Identify five (5) issues or concerns regarding the 159th Street Corridor in Homer Glen

Community members were asked to list the five most important issues facing the 159th Street Corridor in Homer Glen. The following includes an inventory of resident responses as well as a summary of the workshop discussion.

IDOT Road Improvements

A majority of the workshop was spent addressing the impacts the future IDOT road project will have on the Corridor, specifically the installation of the center medians. Issues and concerns included, but were not limited to, unmountable medians, keeping 159th Street open during construction, and the lack of center turn lanes for direct access into private property. A list of IDOT issues identified during the workshop is provided below.

- Traffic signals installed in a timely manner at construction
- Unmountable median
- Semi-truck route
- Installing center turn lanes instead of planted medians
- Review the need for additional signal installations (i.e. Twin Lakes)
- Keep road open during construction
- Cost for construction improvements and

future local maintenance costs

Economic Benefit

There were concerns about competing with surrounding communities (Orland Park, Lemont, Lockport, New Lenox, and Mokena) for commercial development. Residents and business owners agreed that the Village needs to examine ways to create a market as oppose to “stealing” or repeating the development patterns of surrounding communities.

Incentives were recommended, such as water and sewer hook-ups, to support existing businesses and to also encourage new development along the Corridor. A list of economic benefits and issues identified during the workshop is provided below.

- Competing with surrounding communities to develop a successful commercial corridor
- Available sewer and water hook-ups for future development

Image & Identity

Creating a distinct look and feel along the 159th Street Corridor is critical to changing the perception of the Village. An attractive appearance and a clear sense of place is an important part of drawing shoppers, residents, and businesses to the 159th Street Corridor. A list of image and identity issues identified during the workshop is provided below.

- Need to establish a clear Village identity along the Corridor
- Not allowing electronic signs/billboards
- Planting/landscaping in the right-of-way
- Install Lighting

Access & Safety

There is a concern that after the IDOT expansion occurs, there will be significant safety concerns to deal with. These concerns include managing u-turn traffic patterns, a lack of clear and safe access to businesses and residential neighborhoods, the effectiveness of emergency vehicle responses due to median installations, and how pedestrian mobility will occur along the Corridor. A list of access and safety issues identified during the workshop is provided below.

- Safe and plentiful u-turn access
- Speed of traffic
- Emergency response effectiveness
- Pedestrian crossings
- Access to businesses
- Resident access to and from the 159th Street Corridor
- Install all encompassing safety measures



Question #2

Rank the three (3) most important issues discussed thus far.

After sharing their individual issues and concerns with the group, residents and business owners were asked to prioritize the cumulative list of issues facing the 159th Street Corridor. The list below reflects the priorities as identified during the workshop.

- Installing center turn lanes instead of planted medians (24)
- Access to businesses (23)
- Unmountable median (22)
- Emergency response effectiveness (20)
- Keep road open during construction (16)
- Cost for construction improvements and future maintenance fees (13)
- Lighting (9)
- Planting/landscaping in the right-of-way (8)
- Safe and plentiful u-turn access (6)

- Available sewer and water hook-ups for future development (5)
- Need to establish a clear Village identity along the Corridor (5)
- Resident access to and from the 159th Street Corridor (5)
- Speed of traffic (5)
- Not allowing electronic signs/billboards (4)
- Semi-truck route (3)
- There are too many traffic signals (2)
- Curbs can be dangerous (1)
- Competing with surrounding communities to develop a successful commercial corridor (1)
- Traffic signals installed in a timely manner at construction (1)
- Pedestrian crossings (not ranked)
- Install all-encompassing safety measures (not ranked)
- Review the need for additional signal installations (i.e. Twin Lakes) (not ranked)

Question #3

Identify three (3) specific projects or actions you would like to see undertaken regarding the 159th Street Corridor.

Residents and business owners were asked to identify specific projects or actions that they would like to see undertaken that could address problems along the Corridor. Responses included a broad range of ideas:

- Eliminate unmountable curbs and replace with center turn lanes
- Lower the speed of traffic
- Provide adequate lighting that does not look cheap
- Future traffic light installations based on projected traffic counts rather than current traffic counts
- Examine the current state of emergency vehicle response measures (need additional traffic signals where emergency vehicles frequently access the Corridor)
- Install a sound wall along residential neighborhoods/subdivisions
- Install landscaping in the right-of-way and median

- Provide direct access to bike and walking trails
- Install quality lighting at driveways and intersections on an as needed basis
- Enhance signalized intersections
- Reduce the amount of unnecessary traffic lights
- Encourage pedestrian mobility throughout the Village by beautifying sidewalks, crosswalks, and bus stops
- Implement a “no billboard” zone along the Corridor
- Study alternatives to medians
- Reduce or remove the cost of improvements on residents and local businesses
- Provide access to sewer and water to existing and future development
- Examine the need for signalized intersection or limited turning access at 159th Street/Twin Lakes

Question #4

What specific uses/development would you like to see along the 159th Street Corridor?

Residents were asked to identify what uses/development they would like to see along the 159th Street Corridor. The following comments provide context for the types of uses/development the Village should encourage:

- Sensible mix of retail, office and residential
- Large chain grocery store
- Increase the amount of businesses along 159th Street
- Family friendly businesses
- Municipal facilities village/town center
- Review sign ordinance and provide recommendations to allow for better signage so businesses can be easily identified
- Nice, sit-down restaurants
- Provide direct and easy access to business
- Large chain retail store (Bass Pro, Cabela, and Macy’s)
- Hotel/conference center
- Limit the amount of parking allowed along the Corridor by providing frontage road or parallel road designs for future development
- Capitalize on nearby forest preserves
- Movie theater
- Place buildings closer to the road

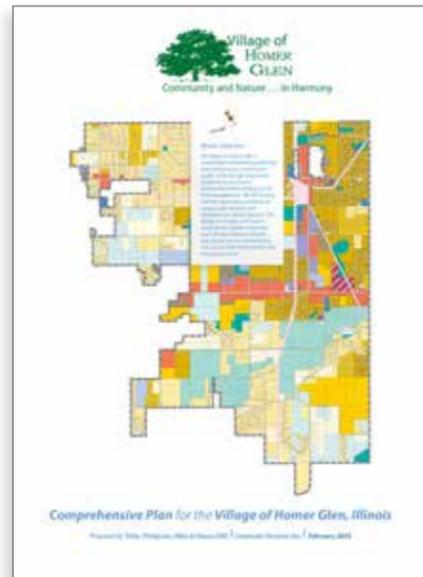
Question #5

What specific uses/development would you NOT like to see along the 159th Street Corridor?

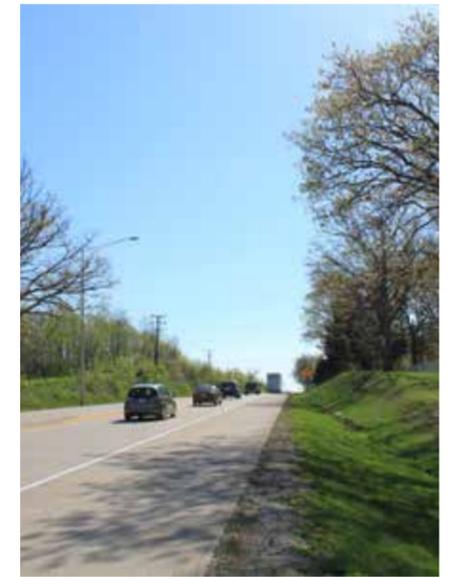
Residents were asked to identify what uses/development they would NOT like to see along the 159th Street Corridor. The following comments provide context for the types of uses/development the Village should discourage:

- Businesses with gravel driveways
- Cheap/dated retail developments
- Industrial/manufacturing
- No red-light cameras
- Unfinished developments/piles of dirt
- Landfills
- Warehouse and storage facilities
- Strip malls
- More bars/sports bars
- Tattoo parlors
- Pawn shops
- Reduce or eliminate semi-truck traffic





Village of Homer Glen's 2005 Comprehensive Plan



Existing Studies, Plans & Reports

The 159th Street Corridor Planning Study and its recommendations provide a level of detail and specificity different than any of the Village's other planning documents. The nature of this type of planning is to build on the general policies and framework of other documents, and provide more detail and strategic actions and recommendations. Homer Glen's 159th Street Corridor Planning Study builds upon, and is informed by, the following plans.

Village of Homer Glen Comprehensive Plan – February 2005

The Village's Comprehensive Plan, adopted in 2005, sets forth long range recommendations for future growth and development as well as the enhancement and conservation of the existing image and character of the community. It stresses the preservation of natural resources, open space, and the rural-residential character of the Homer Glen community. Given the focus on "green" thinking, The Green Vision of the Homer Glen Community (Page 8) is included and referenced throughout the document. The Plan was compiled through an extensive, 16-month process that engaged both public and private stakeholders.

In order to expand the Village's tax base, the Plan also encourages strengthening development along existing mixed use and multi-purpose commercial and industrial spines such as 159th Street. To accomplish this, the document includes numerous policies and guidelines pertaining to new development within the Village, many of which have implications for the 159th Street Corridor.

Relevance to This Study

The plan does not specifically focus on the 159th Street Corridor but presents Village-wide transportation related recommendations such as: requiring that commercial/business are served by functional and attractively designed parking and site features; working with IDOT to prepare design recommendations for arterial routes; creating distinctive identity gateway treatments; limiting truck movements and through traffic on residential streets; promoting tree lined corridors with sidewalks, paths, and lighting, and burying utilities where costs allow. The plan also promotes adjacent development internal circulation systems along major corridors that minimize driveways and parallel frontage roadway systems and improve circulation on the Village's major corridors.

Village of Homer Glen Parks & Recreation Master Plan March 26, 2013

Prepared in 2010 and updated in 2013 to accommodate for the Village's increasing residential development, the Homer Glen Parks and Recreation Master Plan is a policy document intended to preserve and improve existing open space, provide guidelines for new park development, and market the community's recreational assets.

The 94-page planning report sets forth long-range recommendations for land acquisition, development, and enhancement of parks/open space within the community and provides recommendations regarding recreational opportunity for residents.

As it relates specifically to the 159th Street Corridor, the Master Plan recognizes 159th Street as a major commercial district with opportunity for recreation.

The following parks and open spaces are located either adjacent to or nearby the 159th Street Corridor, from west to east:

- Fiddymet Creek Preserve (plans for proposed trail loop)
- Morris Park (active recreation, no trails)
- Stonebridge Woods Park (active recreation)
- Messenger Marsh Preserve (3.5 miles of limestone trails)
- Lamers Park (passive recreation, no trails)

Relevance to This Study

The plan promotes the development of attractive and usable parks and open spaces within the Village's commercial areas and encourages the "greening" of major street corridors. The plan also promotes the continuation of a multi-use trail, at least ten (10) feet wide, as an alternative to motor vehicle travel to sites of interest within the Village. The plan also suggests the investigation of large area utility easements for low-intensity recreational uses. The plan identifies pleasure walking and bicycling as the top two most popular outdoor recreational activities within the Village of Homer Glen.

159th Street Corridor Plan – June 2010

The 159th Street Corridor Plan, completed in June 2010, provides a planning guide and overview for establishing convenient and efficient access to Corridor businesses and civic amenities for both local and regional visitors. The multi-modal facility will support mobility options that reduce automobile dependency while establishing a vibrant and attractive image for the communities along the corridor.

Relevance to This Study

The plan presents many recommendations to improve mobility such as providing signal interconnection, pedestrian crosswalks, far-side bus stops, multi-use paths, pedestrian islands, pedestrian priority paving at driveway crossings, reduced number and width of curb cuts, shared site access, and provide internal connector/circulation roadways and barrier medians. The plan also outlines methods of improving Corridor attractiveness and image, which include consolidating or burying utility lines, providing consistent dark-sky type street-lighting for automobiles and pedestrians, and providing landscaped medians. U-turns at intersections to counter access restrictions due to median barriers is also discussed.

IDOT Strategic Regional Arterial (SRA) – February 1995 & 2012 SRA303 Update

The SRA documents established design criteria, planning guidelines, and mobility recommendations for over 1,300 miles of roadways in NW Illinois. The US 6 (159th Street) corridor between IL Route 59 in Will County and IL Route 83 in Calumet City has been identified as a SRA Route. The objectives of the SRA program are to provide a network, in combination with other SRA's and Expressways, to carry high-volumes of long-distance traffic.

Relevance to This Study

The US 6 SRA corridor plan overlaps a portion of 159th Street between Cedar Road and Will-Cook Road. The plan presents many recommendations to improve the Corridor includes expanding the roadway to 4-12' lanes with an 18'-30' median; managing access with right-in/right-out (RI-RO) access and limiting median breaks to 1/4-mile spacing; adding, improving, and synchronizing traffic signals; promoting park and ride facilities especially at Interstate 355; and reserving space for future bus stops/bus shelters/bus turn-outs at Gougar, Cedar, Parker, Bell and Will-Cook Roads.

Existing Land Use & Boundary Agreements



Legend

Existing Land Use

- Single Family
- Single Family Attached
- Multi-Family
- Commercial
- Office
- Medical
- Industrial
- Education
- Governmental
- Public/Semi-Public
- Park
- Open Space
- Agriculture
- Utility
- Vacant

Relevant Boundaries

- Homer Glen Village Limits
- Lockport Village Limits
- Lockport Jurisdiction
- Municipal Boundary Agreement

Village of Homer Glen Transportation Plan – March 2007

The primary objective of the plan is to provide a coordinated sustainable system of roadways, pedestrian facilities, recreation pathways and public transportation services that promote safe and efficient movement of vehicles, pedestrians, goods, services, and emergency responders while enhancing the countryside character and environmental amenities of the Village.

Relevance to This Study

The Village's 2007 Transportation Plan reiterates many of the guidelines and mobility recommendations of the IDOT SRA Report. The Village's Transportation Plan mentions that the most accident prone locations on 159th Street are at the intersections of Bell Road, Cedar Road, and the segments of 159th from Parker Road to Will-Cook Road.

However, the Village report conflicts with the SRA report in that it recommends intersection spacing at 1/8-mile intervals rather than 1/4-mile. The Mayor and staff worked with IDOT, earlier in IDOT's planning process for 159th Street improvements, to increase median break spacing to about 1/8 mile, rather than using the 1/4 mile spacing guideline in IDOT's SRA guidelines.

The SRA report also conflicts with the Village of Homer Glen's Comprehensive Plan, recommending a parallel reverse frontage roadway system for 159th Street.

Lockport Interstate 355 Corridor Study – June 2008

The Lockport Interstate 355 Corridor Study outlines the City of Lockport's long-range plan for improvement, development and growth for the Corridor. The plan establishes a foundation for future decision-making regarding land-use and development, and transportation and circulation. The plan is intended to guide appropriate, market viable, and context sensitive development in all areas of the Corridor.

Relevance to This Study

The Lockport I-355 Corridor Study identifies 159th Street as a heavy commercial and retail Sub Area for the City of Lockport. It recognizes several areas with development potential near the interchange of Interstate 355 and the 159th Street Corridor along the eastern edge of the highway. The Corridor Study also suggests an entertainment district at the intersection of 159th Street and Gougar Road. Although the plan does not specifically address trails along 159th Street, the Plan does promote connections between future off-road trails north and south along Interstate 355.

CMAP Go To 2040

The Go To 2040 Plan, completed by the Chicago Metropolitan Agency for Planning, is a comprehensive regional plan that seeks to maintain and strengthen the region's economic, environmental, and general livability through leveraging human capital, creating a more efficient government, and enhancing regional mobility.

As a big picture policy document, the plan sets forth land use, transportation, housing, environmental sustainability, economic development, and local government finance recommendations for government officials to consider when conducting planning efforts.

Relevance to This Study

For the Corridor, the Go To 2040 plan serves as an overarching reference document for transportation and mobility and land use and housing.

Green Vision of the Homer Glen Community

In 2002 the Village of Homer Glen was chosen as one of sixteen communities in the State of Illinois to receive a grant for the Governor Ryan's Green Communities Demonstration Program. This grant, administered by the Illinois Environmental Protection Agency, was to undertake an innovative visioning process to protect the environment and enhance community well being. The visioning process was held between 2002 and 2004 and involved the community and stakeholders concerned with the future of Homer Glen.

Relevance to This Study

The Green Vision Plan sets forth a number of goals that align with the goals and considerations of the 159th Street Corridor Planning Study, including the protection and enhancement of open space, the preservation image and identity of Homer Glen, and the establishment of a coordinated system of roadways, pedestrian facilities, recreational pathways, and public transportation.

Chapter 2

Market Conditions of the Corridor

This analysis examines the 159th Street Corridor's competitive position within the market. Market data is reconciled with existing conditions to assess whether parcel size(s), traffic flow/circulation, and access within the corridor can accommodate those uses identified. As a residual benefit of this analysis, some market opportunities that may be better suited for other locations within the Village are also identified.

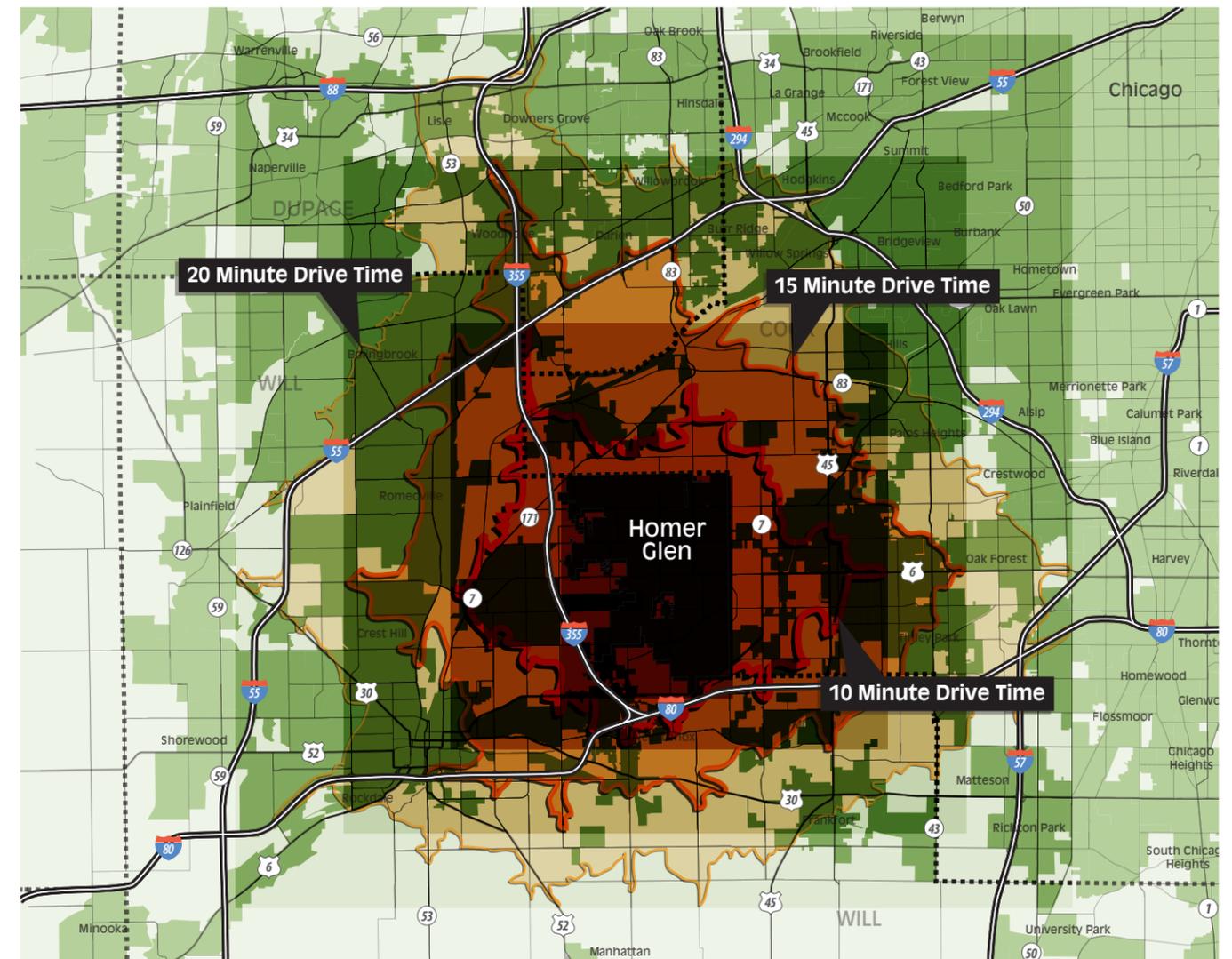
It is important to note that this analysis is specific to the 159th Corridor. Whereas market demand and market opportunities may exist, they may not be appropriate or suitable for the study area.

Defining the Market Area

Defining market areas is highly dependent on a site or corridor's surrounding environment and the context within which it functions. In urban areas and more densely populated suburban areas, consumers typically travel a shorter distance for convenience and neighborhood oriented retailing such as grocery stores. In locations such as Homer Glen, consumers are more conditioned to traveling further distances given the geographic spacing of retail nodes. Whereas the market for certain convenience uses may be 5 to 7 minutes in a more heavily populated area, it may increase to 10 to 12 minutes in a more rural setting. Individuals with a drive time of 10 minutes or more from the corridor will visit area shops and restaurants less frequently or on a more targeted basis.

In conducting this analysis, the market area was reviewed under three separate drive time scenarios (10, 15, and 20 minutes). Proximity and access to Interstate 355 significantly expands the geographic market area. Ten minutes extends as far north as Interstate 55 and south to Interstate 80. East to west includes Lockport and Orland Park. 15 minutes extends north past Woodridge and south to New Lenox and Mokena and 20 minutes as far north as Interstate 88 and south of the communities of Frankfort and Rockdale.

The larger market areas are shown more for illustrative purposes, and to demonstrate how proximity and access to interstates can facilitate people into and out of the area. While individuals will certainly travel from beyond the 10 minute drive time to patronize businesses along the corridor, it will be a smaller percentage in comparison to the 15 minute drive time which represents a more realistic demonstration of actual potential. For purposes of this analysis, the 10 minute drive may be referenced as the "primary" market and the 15 as "secondary" market. The following map depicts the respective market areas.



Drive Time Map

Source: U.S. Census; ESRI Business Analyst; Houseal Lavigne Associates

Demographic Overview

Demographic data has been assessed to identify how commercial development within the market area will be impacted by shifts in the local population. This overview examines anticipated shifts in population and households and changes in the number of households by age and income. Potential impact on demand for commercial development within the market area has also been assessed using this data. Data for this analysis were obtained from ESRI Business Analyst, a nationally recognized provider of market and demographic data.

Residential Population & Households

The following table compares changes in population and the number of households in the market areas. Overall, it is anticipated that both the population and number of households will experience a slight increase between 2012 and 2017. The primary (10 minutes) and secondary (15 minutes) market areas are similar with regard to anticipated population growth and age and income characteristics.

It is projected that the 10 minute market area will experience an annual percentage increase of approximately .51% in population and .56% in households over the five year period. The 15 minute area will grow by several thousand in population and households, however at a slower annual rate of .35% and .47% respectively.

- The median age of the primary market area and secondary market area is expected to remain relatively stable.
- Median household income is projected to increase in both the primary market area and secondary market area at an annual rate of approximately 2%. Although this is slightly lower than the rate for Illinois (3.05%) and the nation (2.55%); the actual median income for the Homer Glen market is significantly higher.

Residential Age Profile

The table to the right illustrates projected population change by age cohort among market area populations between 2012 and 2017. In general, projections indicate that although population growth will occur, growth will be concentrated among older cohorts. The population under the age of 55 will remain stable, experiencing some reduction, while the population aged 55 and over will experience more substantial growth.

- The largest percentage decrease in population is expected to occur among individuals aged 45 to 54.
- Each age cohort above the age of 55 is projected to grow within both the primary and secondary market areas.
- The largest increase in any age cohort is projected to occur among those aged 65 to 74.

Age by Income

Changes projected to occur between 2012 and 2017 are shown as they pertain to each respective household age cohort within respective primary and secondary market areas.

While it is projected that the total number of households will increase, this growth is not likely to occur evenly across all age and income groups. Overall, the proportion of households among lower and middle income groups are projected to decrease while upper income households increase.

- For both market areas, the proportion of households earning less than \$100,000 is projected to decrease, regardless of age. These comparatively lower and moderate income households are projected to decline in both the primary market area and secondary market area.
- Regardless of geography, nearly all age cohorts earning \$100,000 per year or more are projected to increase.
- Households aged 55 and up are projected to be the market areas' most significant sources of growth overall, particularly among households earning at least \$100,000.

Market Implications

Overall, it is anticipated that the population and number of households in both the primary and secondary market areas will experience a moderate increase. In addition, the purchasing power of households within the market area and the proportion of higher income households are projected to increase as is the proportion of older households, which typically have a higher levels of disposable income.

In concert with growing population and household income, data indicate that the spending power of the market area will also increase. When the average household income is multiplied by the number of households, it is estimated that total household income in the primary market area will increase by \$500 million between 2012 and 2017. Within the secondary market area, total household income is projected to grow by \$1.3 billion over the same period. While certainly all of this growth in income will not be distributed within the immediate market area, it does represent potential expenditures to be captured by area businesses.

Population Change & Households

10 Minute Market Area	2010	2012	2017
Population	90,116	90,849	93,183
Households	31,589	31,866	32,775
Families	24,686	24,836	25,423
Average Household Size	2.84	2.84	2.84
Owner Occupied Housing	28,342	28,366	29,365
Renter Occupied Housing	3,247	3,500	3,410
Median Age	40.1	40.5	40.8

15 Minute Market Area	2010	2012	2017
Population	257,229	258,831	263,709
Households	91,112	91,577	93,644
Families	68,763	68,924	70,223
Average Household Size	2.81	2.81	2.80
Owner Occupied Housing	77,608	77,250	79,684
Renter Occupied Housing	13,504	14,327	13,960
Median Age	39.4	39.9	40.2

Source: U.S. Census; ESRI Business Analyst; Houseal Lavigne Associates

Population Change by Age/ 10 Minute Market

Population by Age	2010		2012		2017	
	Number	Percent	Number	Percent	Number	Percent
0 - 4	5,291	5.9%	5,315	5.9%	5,458	5.9%
5 - 9	6,323	7.0%	6,325	7.0%	6,450	6.9%
10 - 14	6,974	7.7%	6,906	7.6%	7,105	7.6%
15 - 19	6,782	7.5%	6,511	7.2%	6,345	6.8%
20 - 24	4,595	5.1%	4,719	5.2%	4,450	4.8%
25 - 34	9,336	10.4%	9,582	10.5%	9,924	10.7%
35 - 44	12,334	13.7%	12,109	13.3%	12,058	12.9%
45 - 54	15,629	17.3%	15,278	16.8%	14,418	15.5%
55 - 64	11,729	13.0%	12,348	13.6%	13,212	14.2%
65 - 74	6,113	6.8%	6,600	7.3%	8,171	8.8%
75 - 84	3,535	3.9%	3,567	3.9%	3,856	4.1%
85+	1,478	1.6%	1,590	1.8%	1,735	1.9%

Source: U.S. Census; ESRI Business Analyst; Houseal Lavigne Associates

Population Change by Age/ 15 Minute Market

Population by Age	2010		2012		2017	
	Number	Percent	Number	Percent	Number	Percent
0 - 4	15,402	6.0%	15,437	6.0%	15,723	6.0%
5 - 9	17,558	6.8%	17,529	6.8%	17,778	6.7%
10 - 14	19,043	7.4%	18,826	7.3%	19,292	7.3%
15 - 19	19,160	7.4%	18,357	7.1%	17,791	6.7%
20 - 24	14,526	5.6%	14,857	5.7%	13,891	5.3%
25 - 34	29,060	11.3%	29,687	11.5%	30,380	11.5%
35 - 44	33,588	13.1%	32,874	12.7%	32,569	12.4%
45 - 54	42,378	16.5%	41,355	16.0%	38,827	14.7%
55 - 64	33,520	13.0%	35,182	13.6%	37,356	14.2%
65 - 74	17,915	7.0%	19,301	7.5%	23,639	9.0%
75 - 84	10,365	4.0%	10,410	4.0%	11,090	4.2%
85+	4,711	1.8%	5,015	1.9%	5,373	2.0%

Source: U.S. Census; ESRI Business Analyst; Houseal Lavigne Associates

Employment & Business

As with the residential population, the number of employees and businesses within the market also impacts development potential. Within a ten minute drive of the corridor there are approximately 5,000 different businesses with over 27,000 employees. Expanding to fifteen minutes the number of businesses increases to over 14,000 with nearly 100,000 workers. The twenty minute drive time contains nearly a quarter million employees. Although many of these people live and work in the same market a high percentage commute. While this study does not analyze the commute pattern of market area employees many pass through or near the 159th Street Corridor daily. This represents an additional potential market to capture.

Employee & Business Information

	0 - 10 minutes	0 - 15 minutes	0 - 20 minutes
Total Businesses	5,033	14,372	35,391
Total Employees	27,041	97,089	240,871
Total Residential Population	90,849	258,831	677,734
Employee/Residential Population Ratio	0.30	0.38	0.36

Source: U.S. Census; ESRI Business Analyst; Houseal Lavigne Associates

Retail Market

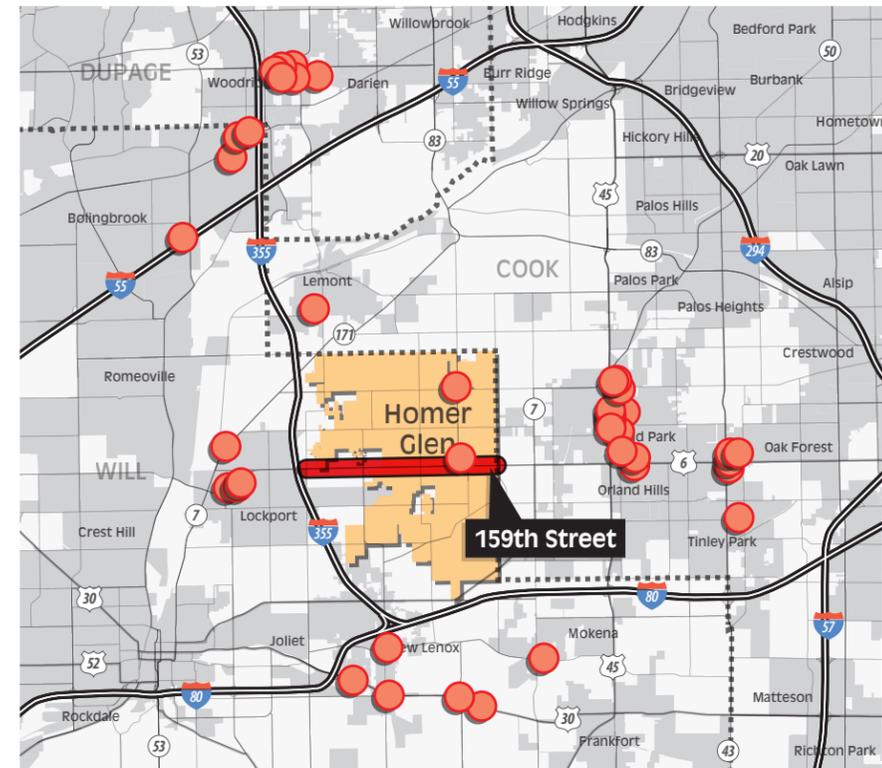
Retail Competition

As previously noted the market area has been delineated by drive time from the 159th Street Corridor. In addition to understanding the demographics, commercial supply/demand and other factors, understanding the competitive retail context within which potential development would compete is an important factor. The following map depicts some of the competitive retail concentrations near the 159th Street Corridor. Most notably large retail nodes exist to the north at Interstate 355 and Interstate 55 including the Bolingbrook Promenade and Orland Square in Orland Park.

Within Homer Glen itself, other areas such as 143rd and Bell Road are already well positioned to attract commercial potential. In addition, there are existing retail centers on 159th Street that should be addressed, in terms of reinvestment. Careful consideration should be given to ensure that new commercial development does not compete with existing uses.

Traffic Counts, Access & Visibility

National retailers outline a specific set of standards when evaluating a potential site. One determining factor is a location's minimum Average Daily Traffic (ADT). The ADT figure measures the average amount of traffic on a street on any given day. Larger retailers typically look for an ADT count of between 20,000 and 30,000 when deciding if a particular site is well suited for future development. Given these criteria, commercial development should be positioned at key intersections along the corridor. Current traffic counts along 159th Street, according to the Illinois Department of Transportation (IDOT), are between 15,000 and 20,000 per day with traffic counts peaking at Bell Road, where the intersection witnesses around 20,400 vehicles per day.



Key Retail Locations

Source: U.S. Census; ESRI Business Analyst; Houseal Lavigne Associates

Retail Gap Analysis

The following table highlights the supply and demand for key retail categories. The graphs then indicate where there is leakage/surplus to other market areas and those in which there is a surplus within the market area.

Retail Supply/Demand

The following analysis uses a comparison of projected spending by market area households to the existing supply of retail space to assess development potential.

As shown in the following table, in analyzing existing retail supply and demand within the ten minute market area, it is estimated that there is currently an undersupply of retail goods and services totaling approximately \$102 million. The market area had an estimated 31,866 households in 2012 with an estimated retail demand of \$1.26 billion, this equates to a potential expenditure per household of \$39,732, with a current supply of \$36,505 per household. This figure includes all retail, eating and drinking establishments.

In analyzing retail supply and demand within the fifteen minute market area, there is currently an oversupply of retail goods and services totaling approximately \$131.6 million. Within this market area, there are approximately 91,577 households contributing to a current retail demand of \$3.34 billion. This demand equates to a potential expenditure per household of \$36,487, with a current supply of \$37,227 per household.

Leakage/Surplus

The 'gap' analysis provides an indication of "surplus" or "leakage" within a given retail category as shown in the followings graphs.

The presence of a surplus within a given retail category suggests that there is at least enough retail space to accommodate demand for the range of goods and services provided by stores in that category. Conversely, leakage indicates that demand exceeds supply and consumers are spending dollars outside of the market area. This leakage could potentially be recaptured and may represent a commercial opportunity within the market area.

The Leakage/Surplus Factor presents a snapshot of retail opportunity. This is a measure of the relationship between supply and demand that ranges from +100 (total leakage) to -100 (total surplus). A positive value represents 'leakage' of retail opportunity outside the trade area. A negative value represents a surplus of retail sales, a market where customers are drawn in from outside the trade area.

In general, the frequency of visits and size of purchase dictates market area. Smaller frequent purchases, such as groceries, are conducted within a shorter drive time. Consumers are willing to travel further for bigger ticket items such as appliances, although bigger ticket purchases are made less frequently and consumers are more prone to comparison shop.

Typical Market Area

Similar to existing development in the market area, future retail development would most likely serve a combination of customers from both the primary and secondary market areas, performing a dual function of accommodating local residents as well as people from outside of the community.

The 'gap' within the typical market area for a given retailer is representative of demand for that particular type of goods and services.

Meijer

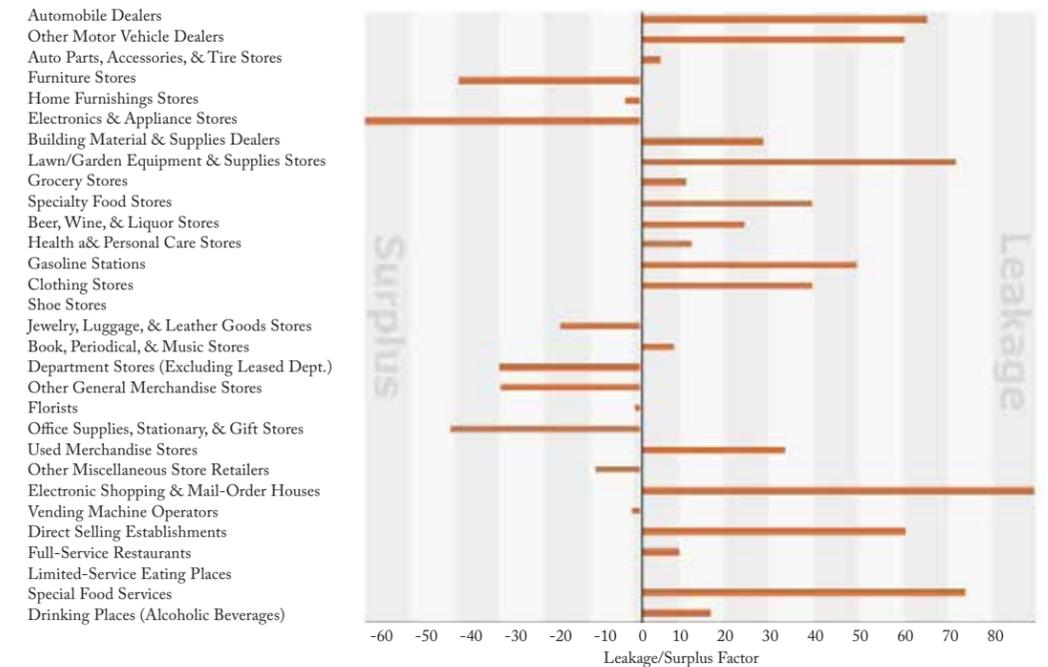
Market supply and demand is continually evolving. In that development potential is based on conditions within a larger area and not just the municipality, updates and modifications should examine the entire market area. For example once opened, the new Meijer store will have an impact on the entire market area not just Homer Glen. Simply adding the store's square footage to the existing supply would not adequately reflect the market unless all other changes and shifts, including demand, are also quantified. ESRI Business Analyst, which was utilized for this market analysis, continuously updates supply and demand information, though there is some delay between the time a store opens and when it is reflected in the data. This is due to the aforementioned need to reassess all market area information including changes in population and consumer expenditures. Therefore, as significant changes occur, the Village should continually reassess its competitive position and potential within the context of the larger market area.

Retail Supply/Demand

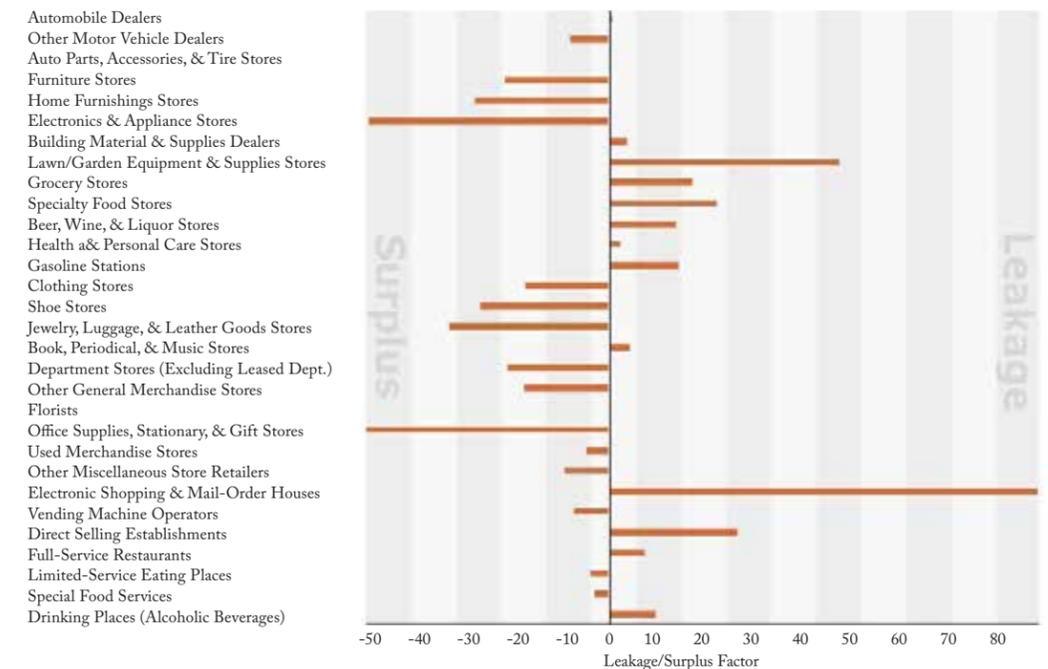
Summary Demographics	Retail Gap by Market Area (\$M)	
	10 Minute	15 Minute
2012 Population	90,849	258,831
2012 Households	31,866	91,577
2012 Median Disposable Income	\$60,910	\$56,329
2012 Per Capita Income	\$35,174	\$32,564
Summary	10 Minute	15 Minute
Total Retail Trade and Food & Drink ¹	\$102.8	(\$131.6)
Total Retail Trade ¹	\$87.3	(\$141.4)
Total Food & Drink	\$15.5	\$9.8
Industry Group	10 Minute	15 Minute
Motor Vehicle & Parts Dealers	\$156.8	\$0.9
Furniture & Home Furnishings Stores	(\$21.7)	(\$43.1)
Furniture Stores	(\$20.9)	(\$21.6)
Home Furnishings Stores	(\$0.8)	(\$21.6)
Electronics & Appliance Stores	(\$105.9)	(\$164.5)
Bldg Materials, Garden Equip. & Supply Stores	\$21.2	\$18.3
Building Material and Supplies Dealers	\$15.7	\$7.0
Lawn and Garden Equipment and Supplies Stores	\$5.6	\$11.3
Food & Beverage Stores	\$38.7	\$141.6
Grocery Stores	\$30.1	\$126.8
Specialty Food Stores	\$3.0	\$5.3
Beer, Wine, and Liquor Stores	\$5.6	\$9.6
Health & Personal Care Stores	\$20.0	\$13.2
Gasoline Stations	\$75.0	\$78.3
Clothing and Clothing Accessories Stores	\$25.0	(\$103.5)
Clothing Stores	\$29.3	(\$57.8)
Shoe Stores	\$0.1	(\$20.5)
Jewelry, Luggage, and Leather Goods Stores	(\$4.4)	(\$25.2)
Sporting Goods, Hobby, Book, and Music Stores	(\$15.0)	(\$34.7)
Sporting Goods/Hobby/Musical Instrument Stores	(\$16.0)	(\$36.2)
Book, Periodical, and Music Stores	\$0.9	\$1.5
General Merchandise Stores	(\$191.6)	(\$252.0)
Department Stores Excluding Leased Depts.	(\$74.2)	(\$109.9)
Other General Merchandise Stores	(\$117.4)	(\$142.1)
Miscellaneous Store Retailers	(\$9.2)	(\$31.9)
Florists	(\$0.1)	(\$0.0)
Office Supplies, Stationery, and Gift Stores	(\$6.4)	(\$22.3)
Used Merchandise Stores	\$1.2	(\$0.6)
Other Miscellaneous Store Retailers	(\$3.9)	(\$8.9)
Nonstore Retailers	\$94.0	\$235.9
Food Services & Drinking Places	\$15.5	\$9.8
Full-Service Restaurants	\$8.9	\$20.9
Limited-Service Eating Places	(\$0.3)	(\$13.2)
Special Food Services	\$5.1	(\$1.0)
Drinking Places - Alcoholic Beverages	\$1.8	\$3.2

Source: U.S. Census; ESRI Business Analyst

Leakage/Surplus Factor by Industry Group- 10 Minute Drive Time



Leakage/Surplus Factor by Industry Group- 15 Minute Drive Time



Retail Potential

There are several categories within which market potential exists. The biggest challenges in accommodating market capacity will be in reconciling land use, physical environment/site capacity, and access.

It is important to distinguish between support in the market and development potential of a specific site or location. The availability of alternative sites, specifications of particular retailers, the number of projects actively pursuing tenants, and similar issues can affect whether market potential translates to development potential.

Figures relating to sales-per-square-foot and typical store size can be utilized to equate consumer expenditures to a preliminary indication of development potential. By dividing a “gap” amount by a given store type’s typical sales-per-square-foot, demand measured in dollars and cents can be effectively translated to a potential number of additional retail square feet that could be supported within the retail market area. If the resulting square footage is within the range of the typical retail format of a given type of retailer, a preliminary conclusion can be made that the market can support additional development in that particular category.

As an example, if it is determined that there is a “gap” in consumer expenditures of \$1 million in a store type that averages \$200 per square foot in sales revenue, then it can be estimated that the market is underserved by approximately 50,000 square feet in that particular category. If the average store for that category is 50,000 square feet, then the market indications are that there is support for one more store. If the average store size is 20,000 square feet, the market could potentially support two to three more stores. Conversely, if the ‘gap’ analysis were to indicate support for an additional 20,000 square feet of retail and the average store size is 50,000 square feet, there is not enough demand to support an additional store.

Store Size & Sales-Per-Square-Foot

Sales-per-square-foot revenues vary by individual retailers and location. However, an examination of benchmarks pertaining to typical sales and store sizes can be applied to yield a better understanding of supportable retail square footage.

Data for prototypical national retailers have been used to benchmark potential square feet of supportable space within a given category. For example, within the primary market area, there is a retail gap of approximately \$8.9 million within the Full-Service Restaurant category (Examples would include Chili’s and Buffalo Wild Wings). With sizes ranging from 2,000 to 5,000 square feet and average sales in the range of \$400 per square foot, indications are that there is sufficient local demand to support one or more new full-service restaurant locations in the market area.

Selected Store Types

Grocery Stores

The typical store size for a chain, such as Jewel, is estimated at between 40,000 and 60,000 square feet. Given the margin between estimated demand and potential, there appears to be ample support for an additional retailer in this category. For many years Dominick’s, Jewel, and a few others have dominated the marketplace. More recently, stores such as Mariano’s and Meijer’s have begun opening locations throughout the suburban market. Other less branded stores with similar formats have also opened in several locations. With Dominick’s recent decision to close in the Chicago market, Jewel has purchased the Homer Glen Dominick’s store. Meijer will open in Homer Glen in summer 2014. The retail gap analysis will need to be updated after Meijer opens to provide relevant information.

Specialty Food Stores

There is an indication of demand for specialty food stores within the market area. Specialty food stores include such retailers as meat, fish and seafood markets, fruit and vegetable markets, and candy/nut stores. Convenience stores are not included in this retail category. These stores have a typical size of between 2,500 and 3,000 square feet within a neighborhood retail setting; however, a national chain in this category, such as Trader Joe’s, has a typical footprint of 8,000 and 10,000 square feet. While Trader Joe’s criteria for site selection is fairly stringent, it does not preclude the potential for another independent retailer, particularly one catering to a specific niche.

Building Materials & Garden Supply

Indications of market potential exist in both the ten and fifteen minute market areas. While uses exist within the market, this means that there is still additional unmet demand.

Restaurants

Potential has also been indicated for eating places in both the primary and secondary market areas, particularly for full-service restaurants. Limited service restaurants appear to be fairly saturated. Full-service restaurants typically require frontage along main thoroughfares and have a typical footprint of between 2,000 and 5,000 square feet. Restaurants, perhaps as much as any other business, are highly dependent on visibility and access.

Limited Service Restaurants

Limited service restaurants include such places as Panera, Noodles & Company, Chipotle, Five Guys and similar establishments. The Village should continue to work with developers, brokers and restaurant representatives to recruit and accommodate these uses where deemed desirable and appropriate.

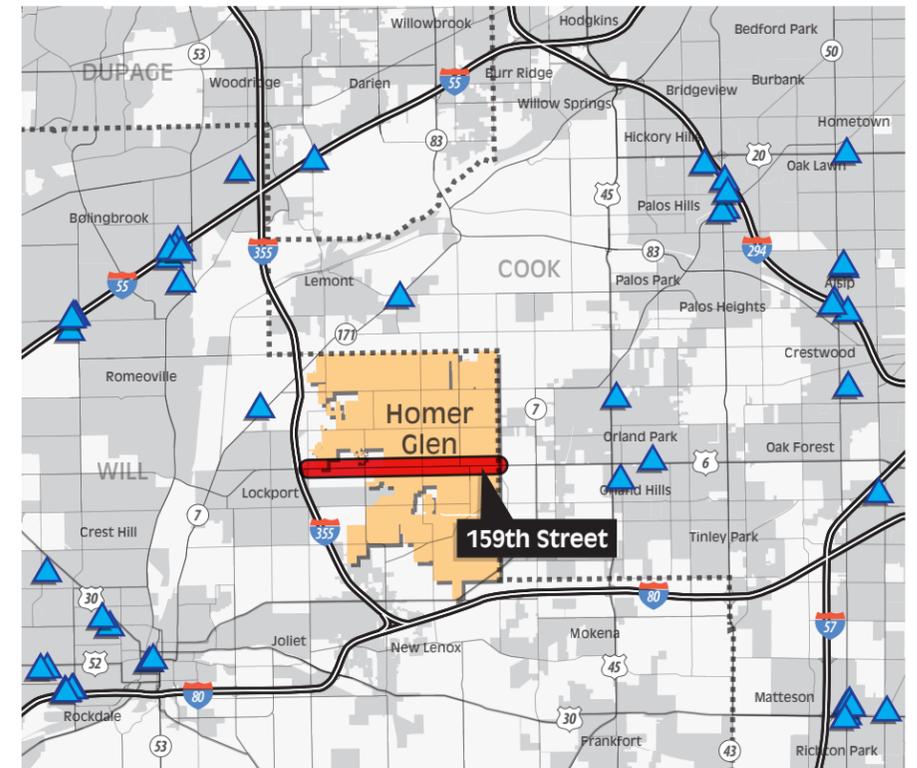
Lodging

Opportunities may exist for locating a hotel property/properties at the intersection of Interstate 355 and 159th Street. This is preliminarily based on the general absence of hotels along the southern extension of 355 between Interstate 55 and Interstate 80. It is not envisioned that hotel properties would be feasible elsewhere within the 159th Street Corridor. A separate hotel feasibility study would need to be performed in order to ascertain potential as well as the number and type of property.

A hotel development would help to define the 159th interchange/gateway and provide additional support for restaurants and ancillary uses. Those uses in turn can anchor the western gateway to the 159th Street Corridor.

Residential/Mixed Use

There is potential within the Homer Glen market for new residential either as a stand-alone development or integrated into a mixed use project. The community and market area’s aging population also represents what will be a growing need for senior housing. It is important to note, however that new residential may be better suited for other areas of the Village. New residential along 159th will need to give careful consideration to traffic circulation, site ingress/egress and compatibility with adjacent commercial uses.



Hotel Locations

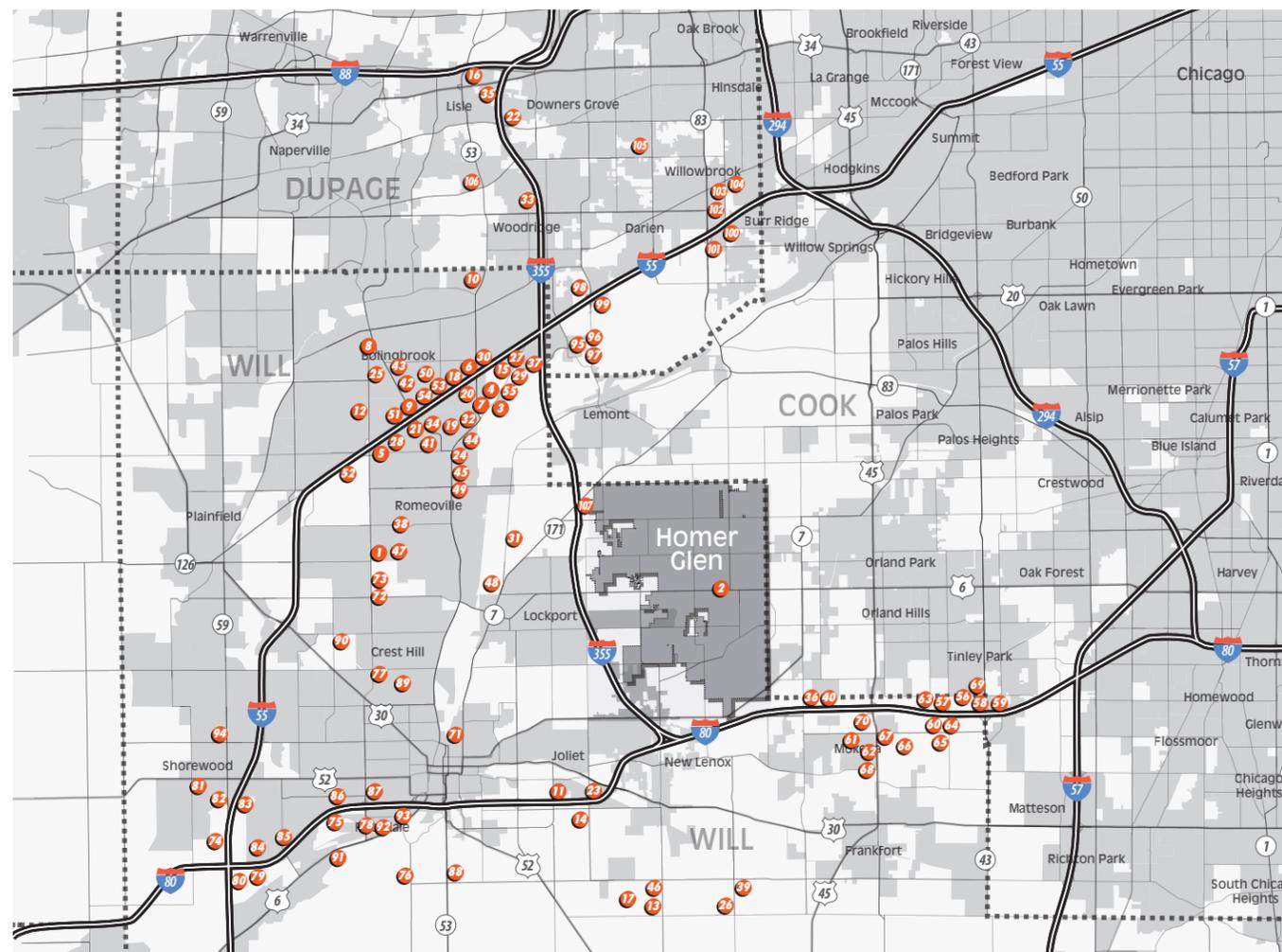
Source: U.S. Census; ESRI Business Analyst; Houseal Lavigne Associates

Office & Industrial Market

Office: Given the amount of available space on the market, speculative office construction is not likely to occur anytime in the near future. Build-to-suit projects will likely be the source of any future development in the near- to mid-term (i.e. Lockport's Heritage Crossings). Professional office space, however, integrated into a larger commercial development may have potential. Medical offices including urgent care related facilities may have potential.

The vacancy rate for the I-55 Corridor and I-80/Joliet industrial markets remain amongst the highest in the Chicagoland market. The reported first quarter 2013 vacancy rates were 11.2% and 12% respectively compared to the regional rate of 9.0%. Short to mid-term industrial related development will be limited while existing supply is absorbed. The caveat is that the southern extension of the I-355 corridor is largely void of office and industrial uses. Sites along arterials with direct access to I-355 may be poised for development sooner than other locations given the geographic juxtaposition of the roadway. Users desiring direct north south access to markets in northern Illinois, Wisconsin and Indiana may find the corridor an attractive alternative to I-294 or I-55 and I-80.

Smaller businesses and those not requiring the infrastructure of a larger business or industrial park, (similar to existing uses) may continue to be well suited for the corridor. It is important to note that those sites should be carefully coordinated to ensure compatibility and synergy and not create conflicts that detract from the overall desirability of the 159th Street corridor locations for other types of development (i.e. retail, restaurants, residential)



I-355 Corridor Business & Industrial Parks

- | | | | |
|-------------------------------------|------------------------------------|---------------------------------------|---|
| 1. Airport Industrial Center | 28. Internationale Centre West | 55. Wood Hill Crossings | 82. Amendodge Business Park |
| 2. Annico Business Park | 29. Joliet Road Business Park | 56. Hickory Creek Corporate Center | 83. Timber Ridge Business Park |
| 3. Bluff Point Business Park | 30. Lily Cache Business Park | 57. Tinley Crossing Corporate Center | 84. Rock Run Business Park |
| 4. Bluff Trails Business Park | 31. Lockport Canal Industrial Park | 58. Tinley Park Corporate Center | 85. Southfield Business Park |
| 5. Boldt Park | 32. Marquette Center Business Park | 59. North Creek Business Center | 86. Oak Leaf Center Business Park |
| 6. Bolingbrook Business Park | 33. MidPoint Corporate Center | 60. Blackhawk Industrial Park | 87. Stryker Avenue Industrial Park |
| 7. Bolingbrook Corporate Center | 34. Naper Crossing Commerce | 61. Highland Commercial Park | 88. Laraway Crossings |
| 8. Bolingbrook Corp. Corridors West | 35. Ogen Corporate Center | 62. Clear-Vue Corp Business Park | 89. Diamond Acres Business Park |
| 9. Bolingbrook Pointe Corp. Center | 36. Orland Park Business Center | 63. North Star Business Park | 90. Crest Hill Business Park |
| 10. Bradlink Industrial Park | 37. Park 355 | 64. Eighteith Ave. Industrial Center | 91. Crown-Trygg Industrial Park |
| 11. Burla Industrial Park | 38. Pinnacle Business Park | 65. Blackhawk East Industrial Park | 92. Alessio Corporation Industrial Park |
| 12. Carlow Corporate Center | 39. Plank Trail Industrial Center | 66. Corporate Corridors of Mokena | 93. Space Corporation Industrial Park |
| 13. Cedar Industrial Park | 40. Precision Industrial Park | 67. La Grange Ridge Business Park | 94. Shorewood Business Center |
| 14. Cherry Hill Business Park | 41. Prologis Park 55 | 68. Normal Towers Industrial Park | 95. International Centre Business Park |
| 15. Corporate Crossings | 42. Prologis Park Remington Lakes | 69. Mercury Business Center | 96. Maple Pointe Business Park |
| 16. Corporetum Business Park | 43. Remington Lakes Business Park | 70. Mokena Crossings Corporate Center | 97. Argonne Business Park |
| 17. Country Creek Business Park | 44. Ridgewood Business Park | 71. Heritage Industrial Park | 98. North Waterfall Glenn Business Park |
| 18. Creekside Corporate Center | 45. Romeoville Industrial Park | 72. Weber Road Business Park | 99. Waterfall Glen Business Park |
| 19. Crossroads Business Park | 46. SkyCorp Industrial Park | 73. Weber Ridge Business Park | 100. Burr Ridge Industrial Commons |
| 20. Crossroads East Business Park | 47. South Creek Corporate Center | 74. Heartland Corporate Center | 101. Hinsdale Industrial Park |
| 21. Crossroads Lakes Business Park | 48. Star Business Park | 75. Moen Avenue Industrial Park | 102. Willowbrook Executive Center |
| 22. Ellsworth Industrial Park | 49. Sunrise Business Park | 76. Port of Will County | 103. Madison Business Park |
| 23. Ferro Industrial Park | 50. Territorial Business Park | 77. Weber-Caton Industrial Park | 104. High Grove of Burr Ridge |
| 24. Hampton Industrial Park | 51. Windham Lakes Business Park | 78. Larkin Commercial Park | 105. Vanudustrial Park |
| 25. Highland Corporate Center | 52. Windham Lakes Business Park SW | 79. Sharp Industrial Park | 106. Hobson Valley Industrial Park |
| 26. Howell Airport Industrial Park | 53. Wood Creek Business Park | 80. National Crossroads Business Park | 107. Heritage Crossings |
| 27. Internationale Centre | 54. Wood Creek Center | 81. Shorewood Industrial Park | |

Preliminary Market Conclusions

The 159th Street Corridor will experience significant changes in terms of road configuration and infrastructure over the next several years. However the nature in which 159th Street functions, in the context of the marketplace, will be similar. The one area that will experience the greatest change will be the Interstate 355 interchange. The area extending from the interchange east to approximately Cedar Road should absorb much of the corridor's new development potential over the next several years.

While other communities and sites do, and will compete for development, there are some benefits of synergy with surrounding development even if that development is located in another community. Development planned in Lockport, to the west of Interstate 355, can serve to create a larger destination environment when paired with development in Homer Glen. Supportable uses will vary depending upon timing, site acquisition, development economics and other variables. Preliminary development potential may include a mix of hotels, retail, restaurants and/or light industrial. All of those uses are well suited for sites with direct interstate accesses. Residential is not envisioned for parcels most proximate to the Interstate 355 interchange, but could be accommodated elsewhere along the corridor.

From a market perspective it is important to note that the corridor does and will continue to function very differently as it traverses east to west. Small industrial/manufacturing uses, residential, and pockets of retail are best suited for sites east of Cedar Road. Retail and professional office should continue to be concentrated at key intersections such as Bell Road. It is not envisioned that commercial development would be supportable throughout the entire corridor at this time. Attempts to foster commercial development throughout the corridor, at this time, would likely result in fragmentation and diluting efforts better suited for other commercial areas within the Village. Destination retailers such as garden supply and landscaping will be appropriate for locations along the eastern segment of the corridor. While this type of use would continue to have market support if located along the western portion of the corridor, the amount of land required would take away from the development potential of uses better suited for sites closest to the interchange.

Ultimately future development will be the result of policy decisions as much as market and site capacity. While development should not be pursued in areas that do not demonstrate market or site capacity, those locations with demonstrated capacity will still need to be endorsed in plans and economic development initiatives. Potential funding sources and development tools that the Village may consider in attracting investment and development at key sites along the corridor are outlined in the implementation chapter of this Plan. In addition, water and sanitary service infrastructure must be brought to the I-355 and 159th Street area to ensure optimal development.

Chapter 3

Land Use & Development Plan

The **Land Use & Development Plan** represents the community's desire and vision for the 159th Street Corridor.

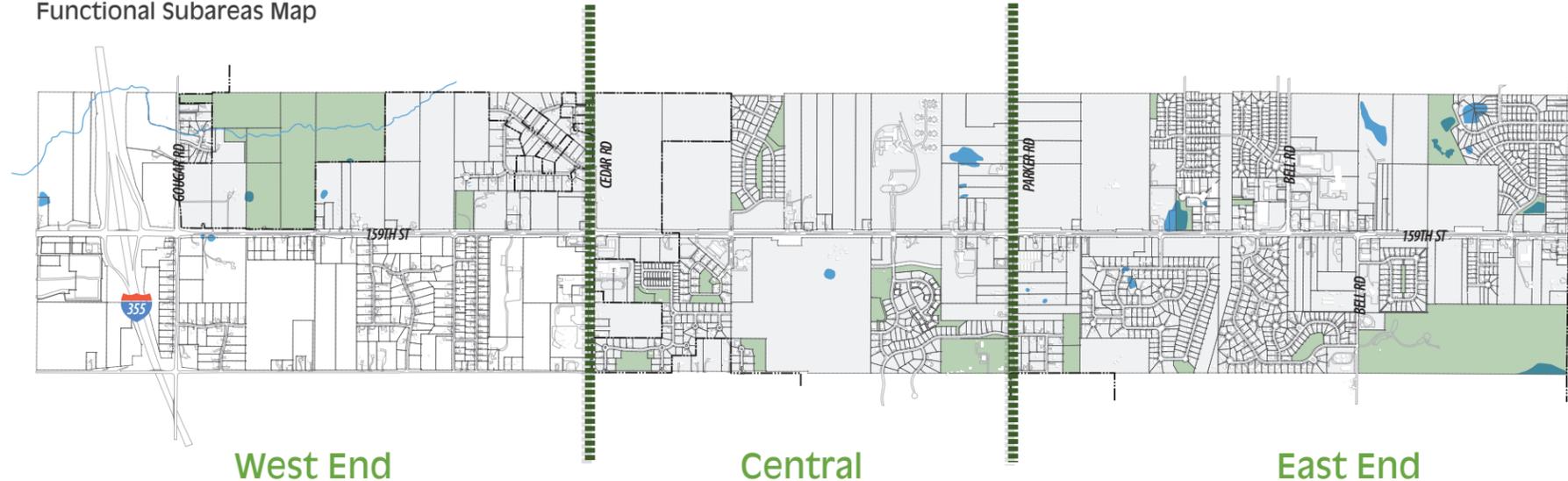
The Plan illustrates what use and type of development is desired for parcels within the Corridor. It suggests changes in land use and possible development scenarios that could be achieved. Site specific development concepts provide a guide for development opportunity sites along the Corridor.

The Plan does not advocate condemnation, but rather seeks to encourage investment in the Corridor through a cooperative relationship with property owners, developers, and IDOT. The **Land Use & Development Plan** for the 159th Street Corridor represents sound planning practices and reflects market realities and community input.

Functional Subareas on 159th Street

Although it is one street, the Corridor consists of several different "functional subareas" along its course, each with a distinct character and capacity for development. The **Land Use & Development Plan** identifies and enhances these different subareas, while unifying and improving the overall appearance, character, and function of the Corridor. Three (3) subareas have been identified along 159th Street – West End, Central, and East End.

Functional Subareas Map



The west end of the Corridor is within close proximity to the Interstate 355 Interchange and should maximize this opportunity with larger scale commercial development that can draw from the wider region. The Corridor Study explores synergies with Lockport's commercial uses to the west and increases the opportunity to diversify the Village's tax base through non-residential land uses and sales-tax revenue generating businesses. Commercial uses in this section of the Corridor should be larger in scale and be designed to draw users from outside Homer Glen.

The central portion of the Corridor may be best described as a hybrid of east and west, comprising a mix of land uses, including commercial nodes at both Bell Road and Parker Road, an industrial park along Annico Drive and single family subdivisions on the south side. The intersection of Bell Road and 159th Street is made up of primarily neighborhood commercial businesses serving nearby residents and employees. In contrast, a business that sells camping gear and a business that sells pools have unique products and draw customers from the larger region. Also within the central segment of the Corridor are sizeable vacant sites, similar to those on the west end of the Corridor, which have the potential to either develop as shallow depth neighborhood commercial with residential uses in the rear; or larger regional level commercial with larger lot depths.

The east end of the Corridor generally consists of lower intensity commercial and office uses, residentially scaled, and compatible with adjacent single family uses. There are opportunities for commercial in the east end but consideration should be given to screen/buffer existing and proposed residential neighborhoods. The east end of the Corridor is also better suited for businesses that cater to nearby neighborhoods.

Land Use Plan Designations

Future land use and development within the 159th Street Corridor is influenced by a parcel's location within the Corridor. Given that the Corridor consists of three (3) distinct functional subareas, the **Land Use & Development Plan** does not designate general land uses applied across all parcels within the Corridor. Instead, the **Land Use & Development Plan** provides recommended land uses that are sensitive to nearby and adjacent uses and in keeping with market realities and community desires.

Residential

The 159th Street Corridor Plan's study area extends approximately ½-mile north and south of the street. Within this study area there are a number of residential neighborhoods situated along, or within close proximity to, the 159th Street Corridor that help to support businesses along 159th Street. The residential areas in the Study Area include three types of residential uses: single family detached, single family attached and multi-family.

Much of the existing residential areas are stable, well maintained, and made up of attractive homes which represent significant homeowner pride and investment. Although these areas are not the primary focus of the Corridor Study, the Village has already taken steps to protect these residential uses from non-residential encroachments and provide for pedestrian infrastructure to connect nearby neighborhoods to shopping and other destinations anticipated along 159th Street.

The Village should ensure that all new, improved, and existing commercial activity and development is effectively screened and buffered from adjacent residential uses by including additional landscaping requirements and appropriate setbacks.

Single Family Detached

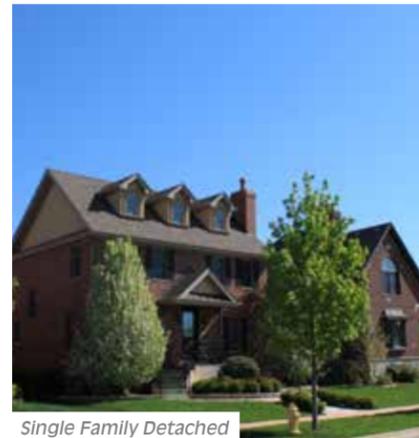
The existing single family detached areas collectively provide a variety of different characteristics, ranging from newer single family detached homes arranged in newer suburban style development to isolated single family residences located on large tree covered lots that front the 159th Street Corridor. In some instances, the **Land Use & Development Plan** recommends changes in land use and the redevelopment of smaller, isolated properties. These properties may be better suited for commercial uses as the Corridor develops over time. Additionally, the **Land Use & Development Plan** identifies opportunities for single family homes adjacent to existing residential neighborhoods. Homes in these new residential areas should not directly front 159th Street but be provided access through consolidated access points. Townhome and duplex residential areas within the Corridor can be used as a transition between single family uses and non-residential uses, while at the same time providing greater housing options in the community.

Attached Single Family

Single family attached residential uses are single family dwellings that share a sidewall with an adjacent single family dwelling. Examples include townhouses, row houses and duplexes. In addition to existing attached single family development, the **Land Use & Development Plan** identifies additional opportunities for attached single family, primarily in transitional areas between potential commercial uses and single family neighborhoods.

Multi-Family Residential

Multi-family residential uses are the densest form of residential development along 159th Street and include apartments and condominiums. There is a relatively small amount of existing multi-family dwellings in the Corridor Study Area. Although the **Land Use & Development Plan** recommends retaining existing multi-family developments, the Plan does not recommend any additional multi-family development within the Corridor.



Single Family Detached



Attached Single Family



Multi-Family Residential

Commercial

Although other types of uses exist along the roadway, 159th Street is primarily a commercial corridor. The Plan reflects the existing commercial uses located primarily in the eastern portions of the Study Area and designates new larger commercial development areas in the central and western portions of the Corridor. Commercial uses along the corridor include retail, service, restaurant, and entertainment uses and range from smaller convenience uses geared toward serving the direct needs of area residents, and also larger commercial developments designed to draw customers from areas beyond the Village of Homer Glen.

Two commercial designations are identified as part of the **Land Use & Development Plan** – Local Commercial and Regional Commercial. These commercial designations are reflective of a potentially strong commercial and retail market within the Corridor. The market analysis undertaken as part of this study indicates that retail/commercial demand exceeds current supply and the result are consumers (Homer Glen residents) spending dollars outside of the market area. This leakage of local shopping dollars being spent in other communities could potentially be recaptured through the development of new commercial uses along the 159th Street Corridor. The high volume of traffic along 159th Street creates a tremendous opportunity for improved commercial opportunities and the large areas of land available for development provide an environment conducive to new development.

Although the corridor represents an auto-oriented development environment, it is important that the corridor and all of the individual developments be pedestrian-friendly. All commercial developments should provide for the safe, attractive, and efficient access and circulation of pedestrian and bicycle activity.

Regional Commercial

Regional commercial areas are concentrated in the western portion of the 159th Street Corridor. These areas have more direct access to Interstate 355 and can more easily take advantage of regional mobility and proximity to the interstate highway system and the existing 159th Street interchange. Regional Commercial uses are larger, more intense commercial uses and are intended to draw customers from throughout the region. Examples include large regional shopping centers and “lifestyle” centers, big box retailers, hotels, restaurants, movie theater complexes, and other service/entertainment-oriented uses.

Local Commercial

Local Commercial uses are located primarily in the central and eastern portions of the Study Area and can represent a wide range of different types, sizes, and intensities of commercial use. Most of the areas designated for local commercial do not have lot depths as deep or parcel sizes as large as the areas designated for regional commercial uses. Local Commercial uses are intended to serve the daily shopping and service needs of area residents, but can also accommodate larger scaled uses that can draw from a slightly larger area, including communities adjacent to Homer Glen.



Regional Commercial



Local Commercial

Institutional

Institutional land uses in the Study Area include Homer Jr. High, Hadley Middle School, Shilling School, Homer Township Fire Department, as well as the Eagle Rock Community Church. These uses provide important community benefits and improve the quality of life for residents. Institutional uses and community facilities add significantly to the desirability and value of the Corridor. The **Land Use & Development Plan** does not recommend any additional institutional uses for the 159th Street Corridor, nevertheless, the Village should ensure that existing institutions are not adversely affected by new development and vice versa.



Institutional

Light Industrial/Industrial

Current light industrial uses located in the Study Area include landscaping companies, storage, auto body shops, and similar service uses. Light industrial and business park uses are appropriate in the five (5) areas shown on the Plan. If these uses remain, buffering and screening with adjoining residential and commercial properties should be improved. Light industrial and business park uses should follow the design and development guidelines provided in this Plan. Redevelopment of these areas over time as office, business park, or commercial uses would be appropriate.



Light Industrial/Industrial

Office

Currently, office uses are mainly located at and around the Bell Tower Office Park off of Bell Road and 159th Street. The market analysis conducted as part of this study indicates that potential may exist for new office space along the 159th Street Corridor, especially if integrated into a larger commercial development. Smaller office uses exist in the eastern and central portions of the corridor along Bell Road and near Parker Road, with the Plan designating new larger office/business park areas just west of the Cedar Glen subdivision, just north of large regional commercial designated areas fronting 159th. This newly designated office site should remain flexible as it may also be capable of accommodating a range of non-retail uses as well. The flexibility will allow for development to respond appropriately to future market conditions.



Office

Parks & Open Space

The Village aims to pursue development that is compatible with the area's natural landscapes. The parks, open space, and recreational areas illustrated in the **Land Use & Development Plan** should assist with maintaining the rural character, while providing recreational opportunities for the community. In addition to maintaining and preserving existing parks and open spaces the proposed IDOT improvements will help facilitate interaction with the Corridor's parks and open spaces with the addition of the multi-purpose pathway on the north side of 159th Street and a sidewalk on the south side. These connections will serve as a base for access to the community's parks and open spaces.

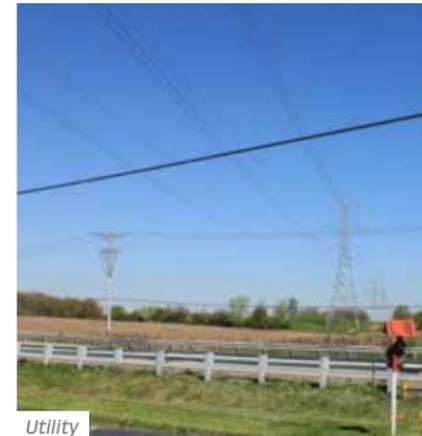


Parks & Open Space

Utility

Com-Ed power lines run north-south through the Corridor near Bell Road. Other utilities in the Corridor include a large cellular tower off of Cedar Road, a power station near the intersection of 159th Street and Gougar Road, as well as an American Water treatment plant off of Bell Road.

The public utility rights-of-way will remain, so redevelopment of sites adjacent to the rights-of-way will need to consider the impacts of existing infrastructure challenges. Utilities are further discussed in the Infrastructure & Utilities chapter of this Plan.



Utility

Agritourism

Agritourism includes any business activity or operation that brings the public to a farm or rural setting to enjoy traditional agricultural uses, or the enjoyment of related outdoor activities. Agritourism allows certain operations relative to farming which are developed to attract tourists and visitors to a particular parcel or parcels of property.

Agricultural Uses

As expressed in the Comprehensive Plan, although not always in the Zoning Ordinance, the Village anticipates that agricultural parcels along 159th Street will someday be developed for other uses. Development of agricultural land will sew the existing irregular land use pattern together to create a coherent commercial corridor. Agricultural land, like the parcel northeast of the intersection of Parker Road and 159th Street, provide an opportunity for new development. Instead of reacting to proposals by developers, this **Land Use & Development Plan** details a vision for the Corridor that provides the Village with a chance to be proactive with future development. To be ready for potential development, the Zoning Ordinance and Zoning Map should continue to be updated to reflect desired development for the existing agricultural parcels along 159th Street, as it has over the past two years.

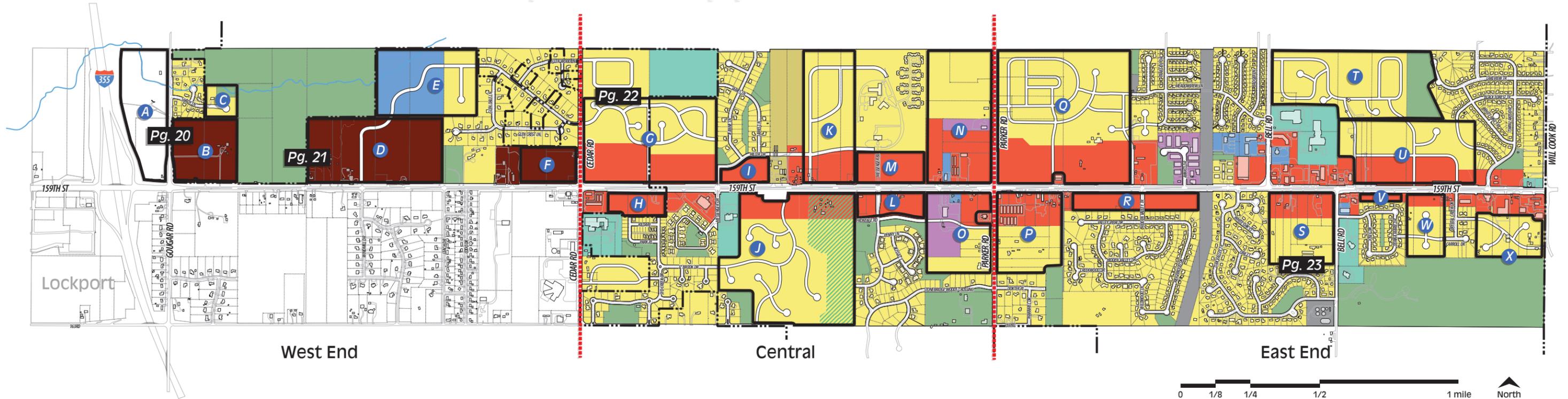
Maintain Flexibility

The Village must maintain flexibility in responding to future development opportunities as market dynamics and conditions shift and evolve. The goal is to facilitate development that serves the local resident population while maximizing larger scale employment and revenue generating uses.

Parcels on the west end of the corridor, most proximate to Interstate 355, have the greatest potential for initial development given the juxtaposition to ingress and egress to the interstate and the potential to create synergy with existing and planned development in Lockport. Development in this area should cater to larger scale retail centers, hotel, light-industrial and/or business/office parks.

The Village should maintain the greatest flexibility as the corridor traverses east. It is important to note that, while much of the corridor is designated commercial, it unlikely that there will be enough market demand to support development throughout all of those areas. Not being overly restrictive initially will maximize the potential to attract catalytic development at multiple locations. As the corridor develops and changes over time and development interest is generated for a variety of uses, the Village can reassess those areas currently designated as commercial. In the interim, maximizing retail opportunities will enhance the Village's tax base and generate activity at key locations. Water and sewer infrastructure must be installed to make this area available for optimal development.

Land Use Plan & Development Opportunities



Legend

- Homer Glen Village Limits
- Residential
- Dunn Farm Conservation Easement
- Institutional
- Regional Commercial
- Local Commercial
- Agritourism
- Office
- Industrial
- Open Space
- Utility
- Development Opportunity
See Page #19 for Site Descriptions

Conflicting Land Use Recommendations

The land use recommendations in Chapter 3 Land Use & Development Plan are substantially different from the recommendations provided in the Village's 2005 Comprehensive Plan. The purpose of this inset is to illustrate those land use conflicts and to provide some context for the direction the Village's growth and development has been heading, and the direction this Plan recommends as the Corridor continues to develop.

Comprehensive Plan for the Village of Homer Glen

2005 Land-Use Plan 159th Street Corridor



- Adjacent Communities
- Single-Family - Estate
- Single-Family - Low Density
- Single-Family - Moderate Density
- Multi-Family Residential
- Commercial
- Business Park
- Commercial - Business Park
- Parks and Open Space
- Agriculture and Related Uses
- Public/Semi-Public
- Public School
- Water Retention Area
- Utility Facility
- Utility Easement/Multi-Use Trail
- Will County Forest Preserve
- Environmental Corridor
- Other Environmental Features to be Preserved

FOR REFERENCE ONLY

Development Opportunity Sites

Site A

This is a prominent development opportunity situated at a signalized intersection at the Interstate 355 exit and entrance ramps. Due to a boundary agreement between Lockport and Homer Glen this parcel will develop in the City of Lockport. The Village should work closely with Lockport to ensure land use compatibly and coordinated and aligned access points from Gougar Road between this site and Site B, which will develop within the Village of Homer Glen.

Site B (see pg 20)

This 40-acre site could represent the best opportunity for development capable of attracting customers and patrons from the larger region. Situated at a traffic signal only 700 feet from I-355, the site has great visibility and access, and has tremendous potential to serve as a catalyst for development and investment in the corridor.

Site C

This site is a 5-acre site north of the Site B, between a single family development and the Fiddymt Creek Forest Preserve. Consideration should be given to using this site for stormwater retention for Site B, which would allow Site B to maximize its development potential. Alternatively, this site could develop with single family detached homes along an extended Shady Lane.

Site D (see pg 21)

This site represents an 80-acre development opportunity at the west end of the 159th Street Corridor, less than a mile from the I-355 interchange. Similar to Site B, this site presents a tremendous opportunity for commercial development with potential to serve as a catalyst for development and investment in the corridor.

Site E

This 60-acre site is bisected by Fiddymt Creek and much of this parcel is heavily wooded. The site is too far removed from 159th Street to yield any significant retail or commercial potential. The environmental setting does provide an opportunity for an attractive campus for professional office buildings or clustered single family homes that would preserve large areas of the site's natural amenities.

Site F

Site F is approximately 20 acres in size at a signalized intersection. The parcel has 1200 feet of 159th Street frontage and a lot depth of 750 feet. Although the site looks linear in comparison with other opportunities in the corridor, its geometry is comparable to the existing Menard's on Bell Road and its depth is sufficient to yield a sizeable development. Development of this site could also consider incorporating existing uses to the west to provide an even larger development opportunity.

Site G (see pg 22)

This opportunity site is one of the largest development opportunities along the corridor. The property adjacent to the north of the site is owned by Lockport Township High School District which acquired the property for a potential high school. Given the sites depth and location, retail uses fronting the entire length of 159th Street should be promoted, utilizing as much depth necessary to accommodate commercial uses. Remaining areas between the future retail uses and potential high school should develop as attached single family and/or multi-family.

Site H

This 13-acre site consists of three parcels that would require assembly to facilitate its comprehensive redevelopment. The site's depth (500'), proximity to Evelyn's North, and better positioned sites to the west make this site not well suited for commercial uses capable of drawing from the larger region. Instead, less intensive, more local commercial uses such as a grocery store that can help provide necessary goods and services to nearby residents should be promoted for the site.

Site I

This 10-acre parcel is an outlot of the Glenview Walk Estates Subdivision and is currently owned by Silver Cross Hospital. The parcel is ideally suited for neighborhood scale commercial uses that cater to nearby residents, including medical uses and professional offices. Development of this site should be closely coordinated with redevelopment of the parcels to the east, and include connections into the adjacent neighborhood.

Site J

Known as the Dunn Farm, this large, 141-acre site contains a number of natural resources, including ponds, large stands of mature trees, and wetlands which will minimize the overall developable area. The site includes a roughly 40 acre conservation easement that may help protect the site's natural features. If wetlands are able to be sufficiently mitigated, the front of this parcel should develop commercially, with clustered single family homes on the rear of the site that could assist in preserving the trees on the site.

Site K

This parcel is located east of Glenview Walk Estates and should develop with a mix of uses sensitive to the existing context of the site. Development should provide an east-west connection between Marian Drive and Hiller Drive. This road would provide a valuable parallel collector north of 159th Street and provide a logical dividing point for development of the parcel, with residential uses to the north, and commercial uses to the south.

Site L

Site L consists of the frontage parcels of the Stonebridge subdivision on both the east and west sides of Stonebridge Drive. The parcels combine for almost 17-acres of land. Given the lot depth, size of parcels and proximity to single-family residential, development should consist of smaller commercial uses catering to the local population and include connections into the adjacent neighborhood.

Site M

Mirroring Site L, this development opportunity is the Marian Village commercial parcel that straddles Messenger Boulevard. Development on these portions of the site could consist of commercial uses, but should complement development on the existing site. Development should also include sidewalks connecting the residents of Marian Village to any commercial frontage.

Sites N O P

These sites are similar in that they contain existing businesses and are located at the intersection of 159th Street and Parker Road. Comprehensive redevelopment of any of these sites would require assembly and considerable cooperation between existing property owners. However, they represent a sizeable development opportunity capable of accommodating a number of uses. The plan emphasizes that any redevelopment of existing commercial uses should come as a result of willing property owners and that the plan does not suggest or recommend condemnation.

Site Q

Site Q is the Gallagher and Henry Site, a full quarter section (1/2 mile square), totaling nearly 160 acres at the northeast corner of 159th Street and Parker Road. The site represents one of the largest development opportunities along the corridor which will be directed by an approved planned-unit development of the site. Owned by Gallagher and Henry, the planned unit development specifies 42 acres of commercial frontage along 159th Street with townhomes and single family homes in the rear of the site.

Site R

The frontage parcels of the Hidden Valley Estates subdivision on both the east and west sides of Hidden Valley Trail combine for 15 acres of land. Given the lot depth, size of parcels and proximity to single family residential, development should consist of smaller commercial and office uses catering to the local population and include connections into the adjacent neighborhood.

Site S (see pg 23)

Prominently located at intersection of 159th and Bell Road, this site currently consists of a mix of smaller uses and much of the property is advertised for sale. To achieve full potential for this prominent parcel, assembly and comprehensive redevelopment is recommended over parcel-by-parcel development. The 159th Street frontage is ideally suited for commercial development, with single family homes or townhomes in the rear.

Site T

North of Hadley Middle School and removed from the 159th Street Corridor, the development opportunity for Site T is residential uses. Clustering of dwelling units should be promoted on this site to preserve the trees, ponds and other environmental features on this site.

Site U

This development opportunity consists of a number of parcels that combine for just less than 80 acres. Parcel assembly and comprehensive development is favored over parcel by parcel development, particularly on the west side of the site where parcels are smaller and narrower. Heavily wooded areas provide potential for an attractive environmental setting for new development, and high-tension power lines that cross the site present design challenges for full utilization.

Site V

This parcel is located on the south side of 159th Street, north of Country View subdivision. The parcel depth, geometry and proximity to single family residential limit its development potential to smaller uses. An office complex has been approved for the site.

Site W

Site W is a deep parcel with some neighborhood commercial potential on its 159th Street frontage. The remainder of the site is best suited for single family residential uses, and should provide for an east west connection of Carroll Drive to the Country View Subdivision.

Site X

This area consists of the undeveloped land between Messenger Woods Forest Preserve and the commercial uses fronting 159th Street. Redevelopment of this site should be limited to residential, including townhomes or single family homes. The development of this site should provide for a connection between Silver Maple Drive and Will Cook Road.

Development Opportunities Alternative Concept Plans

Sites A & B

Site B is a 40-acre site that represents one of the best opportunities for development within the Study Area. The site is capable of attracting customers and patrons from the larger region situated at a traffic signal only 700 feet from I-355. The site has great visibility and access, and has tremendous potential to serve as a catalyst for development and investment in the corridor.

The Village should work with Lockport to develop Site B in conjunction with Site A. Due to a boundary agreement between Lockport and Homer Glen, Site A will develop in the City of Lockport. Site A is a prominent development opportunity situated at a signalized intersection at the I-355 exit and entrance ramps. The Village should work closely with Lockport to ensure land use compatibly and coordinated and aligned access points from Gougar Road between this site and Site B, which will develop within the Village of Homer Glen.



Alternative #1

- Site Area: 54 Acres (Approx.)
- Potential Uses: Regional commercial site to include hotel, theater, restaurants and retail
- Floor Area Range: 400,000-500,000 sq. ft.

Alternative #2

- Site Area: 54 Acres (Approx.)
- Potential Uses: Regional commercial site to include corporate office, hotel, restaurant and retail
- Floor Area Range: 475,000 - 575,000 sq. ft.

Development Opportunities

Alternative Concept Plans

Site D

This site represents an 80-acre development opportunity at the west end of the 159th Street Corridor, less than a mile from the I-355 interchange. Similar to Site B, this site presents a tremendous opportunity for commercial development with potential to serve as a catalyst for development and investment in the corridor.



Alternative #1

- Site Area: 76 Acres (61 Acres Net Open Space)
- Potential Uses: Big box commercial with corridor commercial and professional offices
- Floor Area Range: 325,000 – 425,000 sq. ft.



Alternative #2

- Site Area: 76 Acres (61 Acres Net Open Space)
- Potential Uses: Big box commercial with corridor commercial and office
- Floor Area Range: 500,000-600,000 sq. ft.

Development Opportunities

Alternative Concept Plans

Site G

This opportunity site is a full quarter section (1/2 mile square), totaling 160 acres at the northeast corner of 159th Street and Cedar Road. In terms of size, it is one of the largest development opportunities along the corridor. The property adjacent to the north of the site is owned by Lockport Township High School District which acquired the property for a potential high school. Given the site's depth and location, consideration could be given to locating the future Town Center here. Screening and buffering has been provided along the north edge of Site G since the High School District owns the parcel directly north.



Alternative #1

- Site Area:
 - 101 Acres (Approx. Gross)
 - 97 Acres (Net Gas Line)
- Potential Uses: Big box commercial with corridor commercial transitioning to townhomes
- Commercial Floor Area Range: 300,000 - 350,000 sq. ft.
- Townhomes or Senior Housing: (6 Units/Acre) 330 Units



Alternative #2

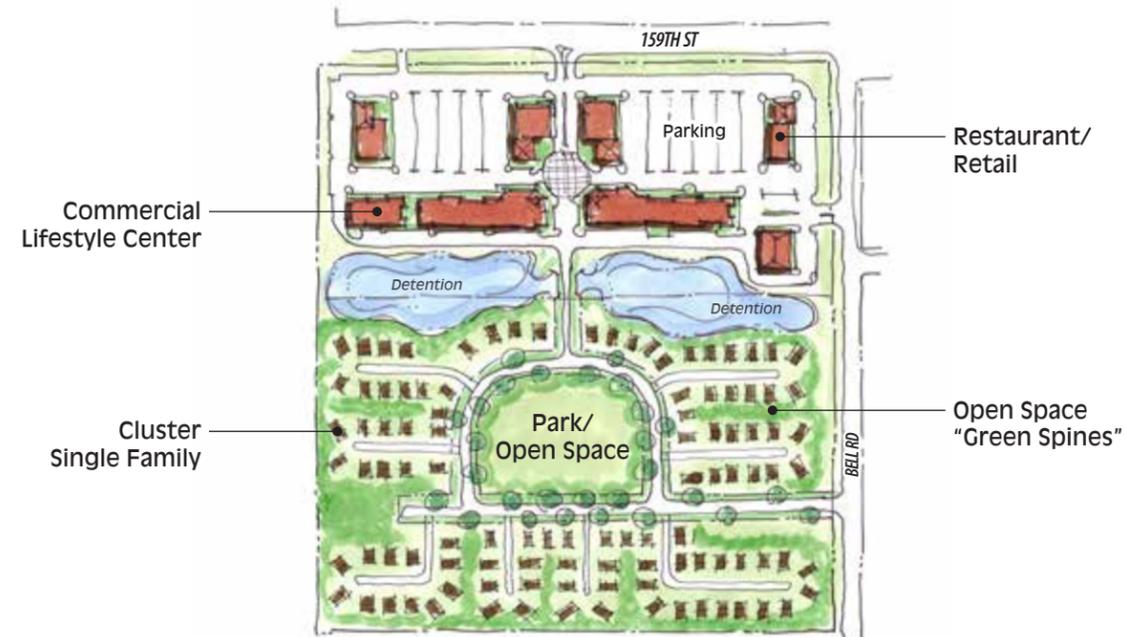
- Site Area:
 - 101 Acres (Approx. Gross)
 - 97 Acres (Net Gas Line)
- Potential Uses: Big box commercial with corridor commercial transitioning to single family cluster
- Commercial Floor Area Range: 300,000 - 350,000 sq. ft.
- Cluster Single Family or Senior Housing: (2 Units/Acre) 110 Units

Development Opportunities

Alternative Concept Plans

Site S

Prominently located at intersection of 159th and Bell Road, this site currently consists of a mix of smaller uses and much of the property is advertised for sale. To achieve full potential for this prominent parcel, assembly and comprehensive redevelopment is recommended over parcel-by-parcel development. The 159th Street frontage is ideally suited for commercial development, with single family homes or townhomes in the rear.



- Site Area: 44 Acres (Approx.)
- Potential Uses: Commercial Lifestyle Center with age-targeted small lot single family
- Commercial Floor Area Range: 125,000 - 150,000 sq. ft.
- Single Family: (3.5 Units/Acre) 91 Units

Chapter 4

Design & Development Guidelines

Articulated since its incorporation and embodied in much of the Village's policy is the desire to maintain Homer Glen's defining rural characteristics. Throughout the 159th Street Corridor Plan's planning process, the desire to maintain the Village's rural character was reinforced and emerged as a common theme. The desire to expand and diversify the Village's tax base also surfaced as a top priority for the community. While these aspirations may seem contradictory, they do not have to be. Economic development can occur without adversely affecting the Village's character. "Community and Nature...in Harmony," the Village's motto, effectively communicates Homer Glen's objective to incorporate urban development within its natural landscape.

Much of the existing development along the corridor was completed under Will County zoning prior to the Village's incorporation in 2001. With the exception of a few recent developments approved by the Village of Homer Glen, most of the development along the Corridor does little to enhance the Village's motto. Application and adherence to the **Design & Development Guidelines** should assist property owners, developers and the Village to accommodate development that improves the Corridor's aesthetics and reinforces and enhances the existing and desired character for the Village.

Purpose of the Guidelines

The **Design & Development Guidelines** address both the public and the private improvements within the Corridor Study Area. The guidelines are intended to support and strengthen land-use, development, and public improvement recommendations found throughout this Plan. In general, the design guidelines strive to accomplish the list below:

- Promote public and private improvements and development that creates and reinforces the desired character and identity for the Village of Homer Glen
- Promote new development that complements the existing development and desired scale and character of the Village
- Improve the appearance and quality of existing sites and buildings within the Corridor
- Ensure high-quality and compatible building and site design throughout the Corridor
- Establish a development pattern that enhances the existing natural resources in the community, and fosters development that is sensitive to, and protective of, the natural environment
- Foster development that respects pedestrian and bicycle activity, while still accommodating automobile and truck traffic

Use of the Guidelines

The guidelines presented in this chapter should be used by the Village in reviewing plans and proposals for all new developments within the community. They should be considered "supplements" to the Village's Comprehensive Plan, Zoning Ordinance, Subdivision Regulations and other applicable codes and ordinances. Architects, property owners and developers are advised to use these guidelines as a reference as they prepare plans for new development projects.

These **Design & Development Guidelines** present basic principles that should be used in improving the overall appearance and character of development throughout the 159th Street Corridor. The guidelines focus on promoting high-quality improvements and developments that will compliment and enhance the overall character of the Village.

The purposes of the **Design & Development Guidelines** are to: a) promote high-quality and compatible new commercial and mixed-use development; b) Improve the design and appearance of existing commercial buildings and properties; c) Improve the image and appearance of the public rights-of-way; d) Promote more design compatibility among buildings and groups of buildings; and e) Establish a distinctive visual image and character for development in the Corridor.

While the **Design & Development Guidelines** focus on new development, they also address the improvement of existing buildings and property. They are based on the premise that even small improvements, if done properly, can have a major impact on the overall appearance and character of individual properties and surrounding environments of the Corridor.

The **Design & Development Guidelines** do not attempt to dictate architectural styles or "make all the buildings look the same." They are not intended to restrict creativity or limit design solutions, but to improve and enhance the overall scale, quality and character of development within the 159th Street Corridor. The guidelines strive to promote a level of quality, compatibility, and consistency that will help make the Village of Homer Glen attractive and distinguishable from other surrounding areas.

It should be emphasized that the **Design & Development Guidelines** are for overall guidance only. Each individual project should be reviewed and considered by the Village on a case-by-case basis.

In addition to these **Design & Development Guidelines**, all new development and building improvements within the Corridor are subject to the appropriate Village codes and ordinances which address the public interest.

Design & Development Guidelines



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Design & Development Guidelines

Commercial, office, and industrial development along the Corridor will contribute significantly to the overall image and character of the Village as perceived by motorists passing through the Village as well as those shopping, dining, or visiting the area. The guidelines for development apply to all existing and planned commercial, office, and industrial properties within the 159th Street Corridor. The guidelines for commercial, office, and industrial development are intended to create attractive and appropriate development that is primarily designed to accommodate vehicular orientation of the strategic regional arterial, while also providing safe and attractive pedestrian and bicycle circulation and amenities.

Building Scale & Design

New development should maintain a “traditional” scale and charm and should provide a unique character for the 159th Street Corridor.

- Buildings consisting of commercial uses should be one to three-stories in height. Building heights should remain flexible to accommodate contemporary development and storage needs.
- A multiple-story architectural element, such as a clock tower, spire, or bell tower, may be appropriate as a design “highlight” at Village approved locations along the Corridor.
- Distinguishing architectural features are encouraged, such as decorative cornices, columns, reliefs, and other facade ornamentation and detailing.
- Covered walkways, such as arcades, are encouraged along the fronts of multi-tenant commercial buildings to create a “pedestrian-friendly” orientation. The columns of the arcade should be aligned with the facade of the building.

- Architectural details should be visible from the street. Buildings should not be setback so far as to diminish the aesthetic impact of the building on passing pedestrians and motorists. Buildings should be attractive at both a pedestrian and vehicular scale.
- Mansard roofs should be prohibited.
- Buildings may have either a pitched or flat roof, but rooflines must compliment the overall design and architecture of the building. Rooflines and parapets should look complete when viewed from all sides of the building.
- Rooftop-mounted equipment and vents should be screened from views along all sides of a building. Rooftop screening should be incorporated into the overall design of the building and be an integral part of the architecture.
- Incorporate all currently adopted Village of Homer Glen Building and Energy Codes.

Building Placement & Orientation

Building placement and orientation objectives should strive to create a development pattern that accommodate both vehicles and pedestrians uses to commercial areas with the 159th Street Corridor Planning Study Area.

- Buildings should have a strong visual and physical relationship to 159th Street to enhance the identity and pedestrian orientation of the Corridor. Buildings should be attractive at both a pedestrian and vehicular scale.
- Although the Corridor is primarily oriented to the automobile, commercial buildings should have a strong pedestrian orientation with display windows, attractive detailing, and convenient and “hospitable” entrances.
- Development should be characterized by building setbacks, side yards and surface parking lots. Consequently, the street frontage should be maintained through the use of low-profile landscaping and decorative walls or fencing.

- All buildings should “front” 159th Street (or Collector Streets) wherever possible. When front doors face north or south, display windows or distinctive facade treatments should be provided along public road frontage.
- Long, blank facades along 159th Street should be avoided. More consideration should be given to retail uses that face pedestrian entranceways.
- All service entrances, dumpsters, loading facilities, and outdoor storage should be located at the rear of buildings; they should be screened from view along sidewalks and roadways through the use of decorative screening, masonry walls, and/or evergreen plantings.
- Wherever feasible, truck access should be designed from side streets and not directly from 159th Street.
- For larger development when appropriate, public gathering spaces should be incorporated as an amenity for the development.

- Window glazing should be clear or slightly tinted and dark, mirrored, or reflective glass is not permitted.
- Buildings within industrial development should not be set back from public roadways so far as to diminish the aesthetic impact of the building on passing pedestrians and motorists.
- Any outdoor seating areas, such as those provided by restaurants or cafés, should be well landscaped and incorporated into the overall site design. Outdoor seating areas should be set back and screened from parking areas and driving aisles.
- Awnings and canopies may be used to provide weather protection and to add visual interest. They should be integrated into the facade and should be in character with the architectural style of the building. Arched or rounded awnings should be discouraged, unless they are compatible with and appropriate to the architectural style of a building.



Building Materials & Colors

The use of appropriate building materials is fundamental to compatible new development and rehabilitation of existing structures.

- Building materials should be of high-quality and durability, and should complement other new buildings within the Corridor.
- Quality materials should be used on all sides of the building, providing an attractive “360° degree” appearance.
- New commercial and industrial buildings should be constructed of traditional masonry building materials such as brick or stone. These materials should be used on all sides of the building. Recommended accent materials include stone, simulated stone, terra cotta, and wood and metal trim.

- Dumpster enclosures should be of masonry construction and should complement overall building design. Dumpster enclosures should be well landscaped and located at the rear or sides of buildings.
- Damaged and deteriorated exterior building materials should be repaired or replaced in a timely manner.
- The predominant colors for new buildings should consist of earth tones. While the natural brick and stone colors should predominate, contrasting and complementary colors should also be used to accent building components, highlight architectural elements and to help distinguish development.

- Color should be used to unite the elements of a facade and to highlight architectural features. However, the colors on individual buildings should complement and be compatible with the colors of nearby buildings.
- For buildings over 20,000 square feet masonry walls may be too cost prohibitive. To ensure high-quality building design for large buildings, variation in building heights, rooflines, and other architectural features should be employed. Facade transparency for buildings should be a minimum 20% of the total area of building walls and landscaping should be installed to break up the monotony of building walls.

Lighting

The Village’s Lighting Ordinance requires full cutoff lighting; promotes the safety and welfare of pedestrians, cyclists and motorists who all benefit from reduction of glare; protects neighbors from unwanted light trespass on their property; eliminates the need for commercial establishments to compete for visual attention by escalating their outdoor lighting levels; and reduces the detrimental effect to wildlife that depend on the natural cycle of day and night for survival. These sound environmental policies benefit residents and serve as a positive example by reducing the amount of energy consumed and natural resources used, money spent on over-lighting, light projected into the night sky, and light pollution. In 2011, Homer Glen was recognized as the 4th International Dark Sky Community in the world.

Lighting can facilitate safe and secure parking and pedestrian areas, and it can also serve to enhance the appearance of a property. Lighting policies for commercial, industrial and office development must be used concurrently with the Village’s Outdoor Lighting Ordinance.

- Lighting may be used to illuminate entries, signage, displays, and pedestrian and parking areas. Any light directed at a building, sign, or other outdoor feature must be located at or above the top of the object and aimed and controlled so that the light is directed only to the object or element intending to be illuminated. Only full cut off fixtures may be used, per the Village’s lighting ordinance. Neon lighting to accent building or architectural features is prohibited.

- Front and rear building entries should be adequately lit for overall security and visibility as long as the lumen per acre budget is not exceeded.
- Building lighting should be subtle and understated; light fixtures must abide by the Village’s lighting ordinance.
- Full cutoff lighting is required on all commercial and industrial buildings.
- Lighting that creates a warm atmosphere must be encouraged. A Correlated Color Temperature of 3000 degrees Kelvin or less is required to create a warm atmosphere.
- Light fixtures should be compatible with building architecture.
- Parking lots within developments must strive to minimize light pollution and glare. The maximum pole height is 25 feet.



Parking Lots

Adequate parking is essential within commercial, office, and industrial development. Parking along the 159th Street Corridor needs to accommodate visitors, shoppers, employees, and residents. Off-street parking lots should be designed and located so that they are safe, attractive and efficient.

- Parking areas of no more than 60 feet in depth should be located between the building and the public rights-of-way. This depth will be sufficient to accommodate a single driving aisle with 90° parking on both sides. Additional parking areas can be provided adjacent to the building on interior portions of the lot.

- The number of curb cuts along the 159th Street Corridor should be minimized. Within commercial development, adjacent parking lots should be connected and access points should be unified. All parking areas should be accessible from cross streets whenever possible.
- Whenever possible, parking lots should be shared between multiple stores and businesses to allow for a more efficient lot layout and to minimize curb cuts. Cross access between adjacent parking lots should also be encouraged.
- All parking areas should be paved or bricked, striped, and have surfaces in good condition. The use of pervious brick paver systems is encouraged.
- Parking lots should be screened from view along sidewalks and roadways through the combined use of free forming berms, low masonry walls, hedge plantings, and shrubs.

- Parking lots should have curbed perimeters and curbed landscaped islands. Clearly marked pedestrian pathways should be provided within the interior of parking areas to avoid large expanses of asphalt and to enhance pedestrian safety.
- Parking lot landscaping should include “landscape islands” at a ratio one per twenty parking spaces. Landscape islands should consist of canopy trees (3 to 4 inches in caliper), attractive groundcover, and/or decorative bushes (2 to 3 feet in height).
- All parking lots should be designed for proper drainage.
- Parking lots that are used during evening hours should be adequately illuminated.

Site Landscaping

Wooded areas and open space contribute to Homer Glen's rural character, and distinguish the Village from neighboring municipalities. Likewise, trees and other landscaping will distinguish the 159th Street Corridor from other commercial areas, and help reinforce the small town atmosphere.

- Native plants to the Northeastern Illinois Region should be encouraged for all landscaped areas within the corridors. Although initially these plants and grasses may be difficult to establish, long-term maintenance costs will be reduced.
- All landscaping within and adjacent to the public right-of-way should be compatible with existing plant materials in the area and be composed of native and salt tolerant species.
- Planters and landscaped areas should buffer parking and service areas. Plantings should consist of low evergreen and/or deciduous shrubs planted in conjunction with low-growing annual or perennial plants and groundcover. Large expanses of exposed mulch are to be avoided.

- A landscaped buffer at least five to eight feet in width should be provided around the perimeter of surface parking lots. Perimeter landscape plantings should include a continuous hedge (3’-4’ in height) accented by trees and ground cover but should not obstruct tenant signage and views to buildings and site features.
- Where site size or configuration does not allow for the minimum five-foot perimeter landscaping, vertical landscaping elements should be employed to provide an effective screen between parking lots and the public right-of-way.
- In conjunction with landscaping, decorative fencing is encouraged to delineate and screen parking and service facilities, outdoor storage areas, etc. Low-profile, decorative wrought iron fencing or masonry walls are recommended.
- Areas adjacent to entrances, monument signs and other site features should be planted with seasonal flowers or colorful groundcover.

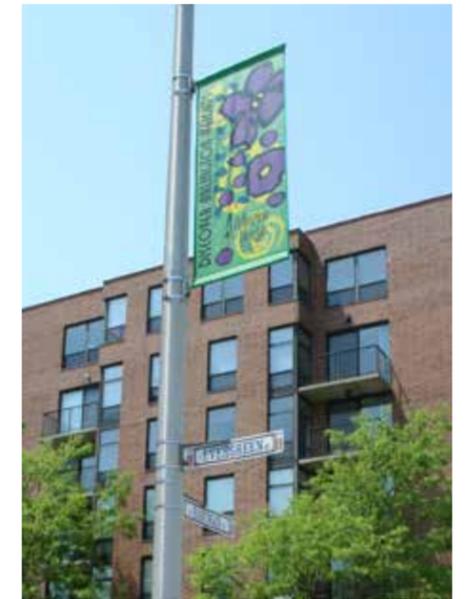
- Evergreens, berming, and other “vertical” landscaping elements should be used to screen intense commercial activity from adjacent residential properties. Masonry walls may be considered as a screening device where landscaping alone is incapable of providing an effective screen. A non-contiguous berm may screen uses and at the same time provide attractive views into commercial development.
- All landscaping should be maintained in a healthy and attractive condition. Maintenance programs should be established as part of new development approvals to ensure that private landscaping is adequately cared for and that its value is retained over time. Regular maintenance should include turf mowing, periodic fertilization, pruning, and the clean-up of litter and debris. Internal irrigation systems are preferred and encouraged.
- Plant material selection and location should take into consideration the visibility of business signs and building entrances. The Village can minimize negative impacts on existing and future businesses by requiring appropriately scaled plant materials along the 159th Street corridor.



Business Signage

Signs not only communicate the nature of individual businesses, but also influence the overall image and character of the Homer Glen.

- Exterior building signs should be limited to business identification and description. The size, material, color, and shape of building signs should complement the architectural style and scale of the building and be integrated as a component of the building's facade.
- Wall-mounted signs are encouraged, although signage should not project above the cornice line or be mounted on the roof of a building.
- Internally illuminated or back lit awnings and canopies, shingle and mansard canopies, and plastic awnings should not be permitted.
- Raised, individual letters mounted directly on the building, as well as signs that use light colors for lettering and darker colors for backgrounds, are preferred. Box signs are not permitted.
- The size of signs should complement facade proportions, and should be constructed from the appropriate scale of the building.
- When a building contains multiple ground-floor tenants, signage for all businesses should be compatible in design and consistent in placement.
- Street numbers should be prominently displayed at the main entrance to every business and be clearly visible from the street.
- Free-standing signage within the Corridor is limited to low-profile monument signs for multi-tenant commercial properties in order to reduce clutter and improve navigation. Monument signs should be attractively landscaped and constructed of traditional building materials similar to the primary building on the site.
- Internal illumination is preferred for building mounted signs. Externally lighted signs may also be acceptable if they are compatible with the requirements of the Outdoor Lighting Ordinance.
- Pole signs, pylon signs, and off-premise signs (billboards) are prohibited in the Village.



Public Improvements

In addition to site and building improvements of development sites, a range of projects should be undertaken within the public rights-of-way and on publicly owned property to enhance the image and appearance of the Corridor to create a safe, attractive, and “hospitable” working and shopping environment. These public sector improvements can also help promote new private investment and development, and attract additional visitors and business patrons to the area.

Sidewalks & Trails

- 159th Street and streets within the planning area should be designed to support vehicles, bicyclists and pedestrians. While accommodating vehicular traffic, streets should also promote walking, cycling, and an overall sense of place.
- Sidewalks should be provided along both sides of all side streets in the Study Area.
- Sidewalks should be designed for pedestrians. All public sidewalks should be a minimum of six feet in width.
- To improve visibility and safety, pedestrian crosswalks within the Corridor Study Area should be made prominent and noticeable by employing a change in paving materials, texture and color.

- All public and private sidewalks within the Corridor Study Area should be accessible to the handicapped and should comply with appropriate ADA (Americans With Disabilities Act) standards.
- Along the Corridor, a “buffer” zone between the walkway and the street should be constructed. This buffer zone should consist of a grass parkway with street trees, light fixtures, and other street furniture.
- Bike paths and access points should be considered as new development occurs.

Pedestrian Amenities

- All new commercial, industrial, and business development should include a unified system of “street furnishings,” such as seating areas, trash receptacles, drinking fountains, bike racks, and other pedestrian amenities. Street furnishings should reflect a traditional design theme, and be consistent with the materials, colors and architectural styles to be promoted within the Corridor.
- Trash receptacles should be placed at key locations within the Corridor. Benches and other pedestrian amenities should be located at key intersections and high activity areas. Bike parking should be provided in visible areas and near main parking areas.
- Bike parking should be provided in visible areas and near main entrances to buildings.
- Open space, courtyards, outdoor seating areas, and plazas should be encouraged in new development projects.

Gateway Signage

- The Village should establish a comprehensive signage system along the 159th Street Corridor to guide and direct both motorists and pedestrians to key destinations.
- “Gateway” signs should be developed at key locations along the Corridor. Signage at Hiller Drive at the western boundary of the corridor and Will Cook Road at the eastern boundary of the corridor are encouraged, as these locations serve as primary entry ways into the Village of Homer Glen. In addition to signage, gateway features should include special landscaping.

- Banners attached to street light standards will distinguish the 159th Street Corridor and could also be used to commemorate special events within the Village of Homer Glen. Banners might be changed periodically during the year.
- The Village’s icon should appear on the full range of way finding signage, directories, and also be used in a variety of marketing and promotional efforts for development along the Corridor.

Chapter 5

Streetscape & Beautification Plan

The purpose of the Streetscape & Beautification Framework Plan is to improve the overall appearance and character of the 159th Street Corridor and enhance its connectivity throughout Homer Glen while maintaining the Village's rural character and landscape. As one of the primary east-west thoroughfares running through the Village of Homer of Glen, it is critical the 159th Street Corridor reflects a positive image of the Village. Currently, from Hiller Drive to Will Cook Road, the 159th Street Corridor contains a wide variety of uses including light industrial uses, both stand along commercial and commercial shopping centers, agriculture land, and single family homes. To enhance the connection of these uses to one another, a series of streetscape improvements and beautification techniques should be employed by the Village of Homer Glen.

Pedestrian Scale

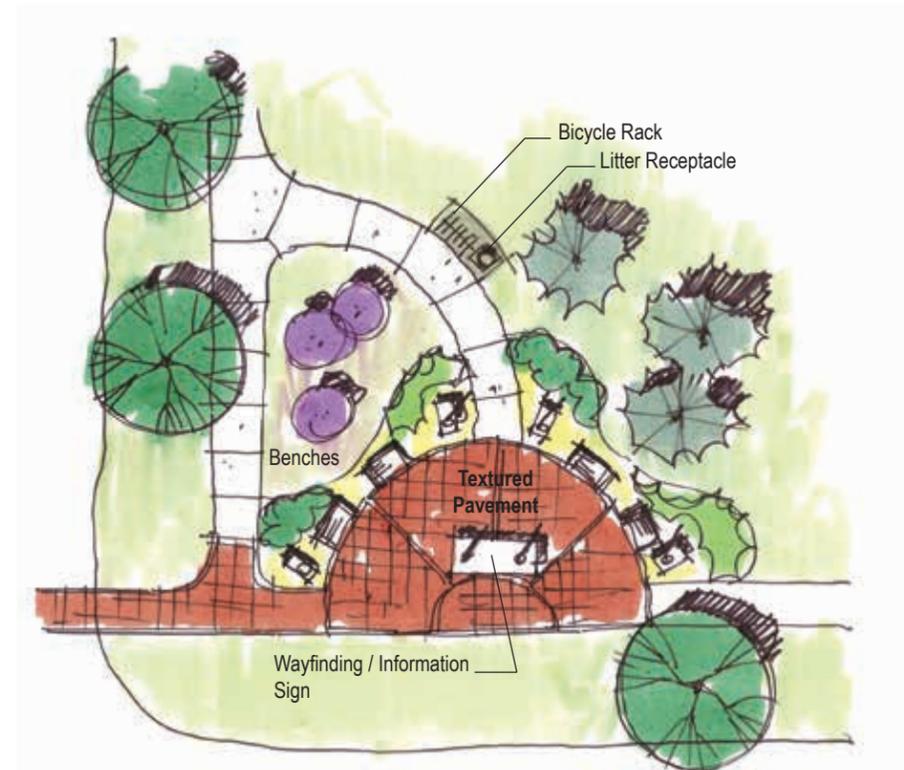
Today 159th Street is an auto-oriented corridor that consists of commercial establishments generating single-purpose visits by automobile. Consideration of the pedestrian is nearly absent, or an afterthought, resulting in a lack of pedestrian scale along the entire Corridor. As it is currently planned, IDOT's widening of 159th Street will include a multi-use "Heroes Trail" path on the north side of the roadway, and a sidewalk on the south side, greatly improving pedestrian mobility along the Corridor. Additionally, IDOT and Village Plans will connect 159th Street's multi-use "Heroes Trail" path to the Village's existing "Heroes Trail" path through a connection to be built in the Com-Ed right-of-way that exists between Bell Road and Annico Drive.

Pedestrian scale improvements should work in conjunction with the multi-use path and sidewalk to create a welcoming pedestrian environment. Improved pedestrian amenities that increase safety and circulation will create the desire for residents to walk or bike to businesses in the area, as well as improve commutes for those who work in the area and arrive by foot or bicycle.

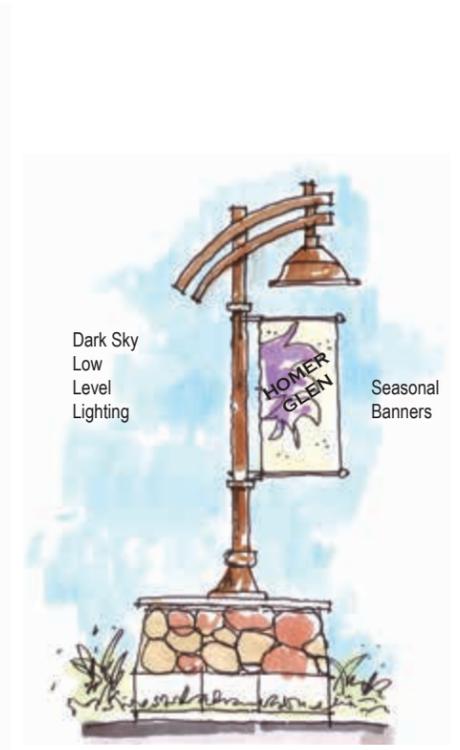
Benches, directional signage, and other such amenities could instantly create a more inviting pedestrian environment as well as establish a theme and character for the corridor to establish a sense of place and community pride. Additionally, the Village should install sidewalks to connect adjacent commercial and residential neighborhoods to the Corridor. In some cases, the Village will need to work with private property owners to obtain the land needed to fill in the sidewalk gaps. Overall, the Village should establish a development pattern that encourages more significant pedestrian and bicycle activity, while still accommodating automobile traffic and parking.

Key Intersection Improvements

Making improvements to key intersections along 159th Street should be a priority of the Village of Homer Glen. Enhancements to 159th Street's intersection with Cedar Road, Parker Road, and Will Cook Road should include decorative crosswalks, attractive landscaping, and signalization. Improving the functionality and appearance of key intersections makes them more attractive to potential developers.



Key Intersection & Pedestrian Plaza



Lighting Standard



Site Furnishing – Benches

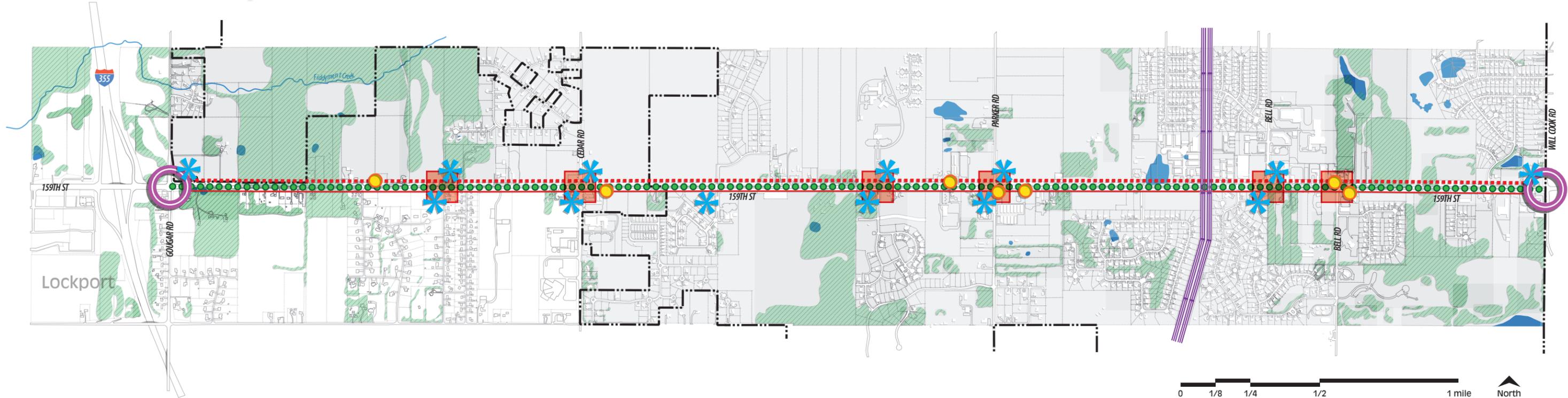


Site Furnishing – Trash Receptacle



Permeable Pavers

Streetscape & Beautification Framework



Legend

- Proposed Gateway
- Wayfinding/Informative Signage
- Key Intersection Improvements
- Streetscape Enhancements
- Remove Billboard
- Preserve Existing Tree Canopy
- Overhead Wires
- Continuous Sidewalk
- Multi-Use Path (Hereos Trail)

Gateway Opportunity

159th Street plays an important role in the regional transportation network. It is regulated by IDOT and is identified as a Strategic Regional Arterial (SRA 306) with a full interchange at Interstate 355. 159th Street's regional significance makes the Corridor a major "gateway" into the Village of Homer Glen for residents, visitors, and motorists passing through.

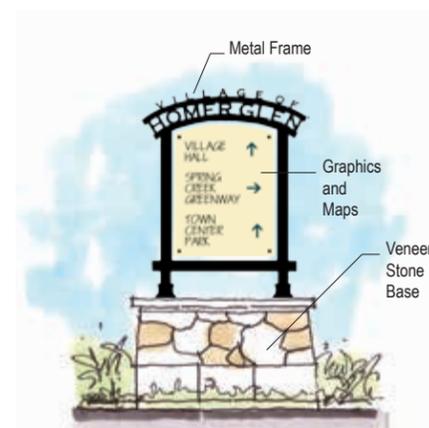
A number of gateway signage opportunities exist throughout the 159th Street Corridor to announce entry into the Corridor and welcome visitors to the Village of Homer Glen. As well as welcoming visitors, gateways also serve to bring a "sense of place" to an area. Strategically located, ornamental, and well-landscaped gateway signs can help the Village Homer Glen achieve a positive presence on the 159th Street Corridor.

Gateways should be placed and oriented to be easily visible for both motorists and pedestrians.

The Village should work with property owners to acquire necessary easements to install new village gateway signs in the areas recommended on the **Streetscape & Beautification Plan**. These areas are specifically near the intersection of 159th Street and Will-Cook Road, and along 159th Street at the Village's western boundary. Specifics regarding the appearance of gateway signage are included in **Chapter 5 Design Guidelines** of this Plan.

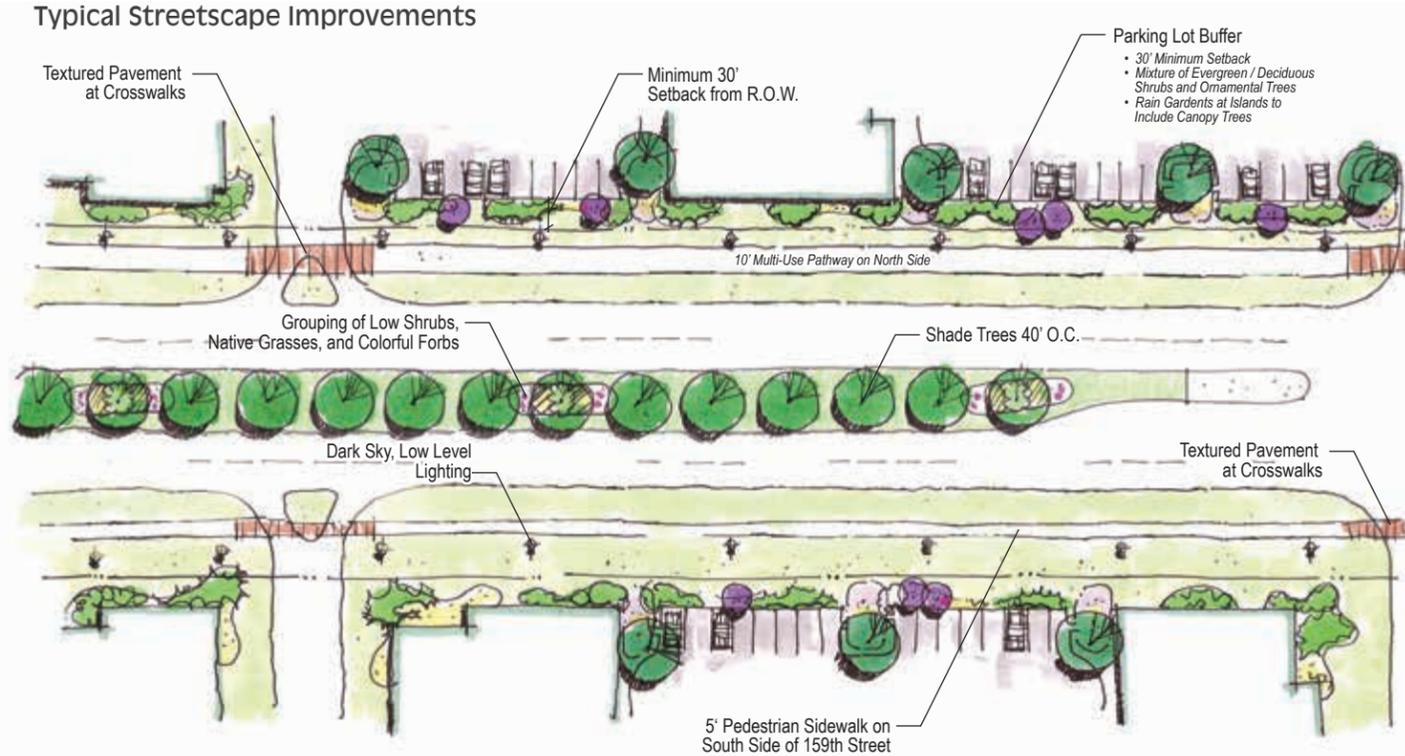
Wayfinding Signage

In addition to gateway features that "announce" arrival into the Village of Homer Glen and 159th Street, wayfinding signage should also be installed to guide motorists and pedestrians to key locations within the Village such as parks, open spaces, event areas, Village Hall, and other municipal facilities. These signs will help improve wayfinding and add to the community's "sense of place."



Gateway & Wayfinding Information

Typical Streetscape Improvements



Streetscape

The Village of Homer Glen should develop and implement a Streetscape Plan, based on recommendation made in this Plan. Installing strategic parkway treatments will act as “early indicators” of the Village’s commitment to beautification of the 159th Street public right-of-way and attract investment. Wooden poles support power lines, and traffic signals are either standard IDOT issue or hung temporarily from wires and cables. Today, there is nothing unique about the 159th Street streetscape that distinguishes this corridor from others, evokes a sense of community pride, or provides an impression of the true character of Homer Glen.

The addition of streetscape amenities such as trash receptacles would improve the cleanliness along the corridor and the installation of benches would provide pedestrians with a place to sit. A unified system of “street furnishings,” that are consistent with building materials, colors and architectural styles would help create a consistent character for the Corridor.

Parkway Landscaping

Parkway landscaping can visually unite a corridor and help establish a sense of place and identity. Parkway landscaping varies significantly along 159th Street. Some owners have invested in landscaping in adjacent portions of the parkway, while others have neglected the parkway all together. In addition to visually uniting the Corridor, parkway landscaping can also play an important role in screening parking areas and reducing noise, light, dust and glare from a roadway onto adjacent properties.

Maintaining a consistent line of street trees along 159th Street where feasible is recommended for the Village of Homer Glen. This goal could be achieved by coordinating efforts with residents and businesses along the 159th Street Corridor. The **Typical Streetscape Improvements** illustrates potential parkway landscaping treatments that can be applied along the corridor. This concept proposes evenly spaced right-of-way trees, flower beds and other improvements but there are numerous options available for the Corridor. Parkway landscaping should be implemented as development occurs along the Corridor, however, median landscaping can be implemented at any time.

Landscaping

Landscaping is largely unregulated in the Village of Homer Glen, particularly along residential and agricultural zones along 159th Street, with the exception of Planned Unit Development (PUDs) that require more extensive landscaping plans. Thus, coordinated site landscaping tends to be more apparent on and along the internal roads and parking lots of residential subdivisions and churches. The Village’s Zoning Ordinance does not provide separate, all-encompassing landscaping standards; landscaping is utilized more as a screening device to separate land uses, control air pollution, or hide loading docks. For example, commercial lots are required to have a ‘front yard’ to act as a buffer between the street and the permitted use.

While screening unattractive uses will significantly improve the image of 159th Street, the Village should consider new **Landscape Guidelines** to convey a consistent landscape appearance along the Corridor. Improvements to the parkway should include sidewalks separated from the roadway by a turf

lawn, parkway trees, understory plantings, and street lighting. Additionally, landscape guidelines should outline specific onsite treatments for business owners. Treatment options should recommend plant types and spacing options.

Due to the fact that many of the properties fronting 159th Street are undeveloped, the **Landscape Guidelines** should provide a phasing strategy to ensure that landscaping is installed after development construction. A phasing strategy will eliminate unnecessary landscaping modifications or removal due to potential roadway access points, site views, and signage conflicts.

Plant material selection and location should take into consideration the visibility of business signs and building entrances. The Village can minimize negative impacts on existing and future businesses by requiring appropriately scaled plant materials along the 159th Street corridor.

Overall, the Village should play a proactive role in coordinating landscape plans between property owners and developers.

General Maintenance, Upkeep, and Beautification

To further beautify and enhance the streetscape of the 159th Street Corridor, the Village of Homer Glen should provide regular general maintenance and upkeep of “fringe” areas between the parking lot of a business and 159th Street. Without this regular “upkeep”, “fringe” areas can become overgrown and unkempt. Trees and other elements of landscaping bordering 159th Street should also be trimmed regularly. Installing more decorative and unique street signage, installing flowerbeds, and repaving or enhancing cracked pavement are just some techniques the Village of Homer Glen could use to beautify the 159th Street Corridor.

Chapter 6

Access & Transportation Plan

Development on the 159th Street Corridor is served by a Strategic Regional Arterial (SRA) route that functions as part of a regional arterial system. As part of a larger system, the roadway is intended to carry large volumes of traffic at higher speeds as a complement to the region's expressway system. Efforts are made to preserve the level of service on these roadways through appropriate access and traffic signal locations and spacing. However, with traffic volumes on 159th Street projected to increase, congestion, delays, air quality and safety are all projected to adversely affect the Corridor and the communities it runs through. To ensure a high level of service for traffic on the SRA route and temper potential adverse effects on the Village of Homer Glen and other south suburban communities, the Illinois Department of Transportation (IDOT) and the Will County Department of Highways (Will County) have proposed improvements to the Corridor (detailed below) that will effectively relieve congestion, mitigate stormwater runoff, and add new modes of transportation to the 159th Street Corridor such as bicycle transportation.

The **Access & Transportation Plan** focuses on coordinating with proposed IDOT improvements to optimize travel within the 159th Street Corridor. The **Access & Transportation Plan** includes specific recommendations for motorized and non-motorized travel, parking, access, circulation and linkages between key areas of the 159th Street Corridor and surrounding areas. The Plan identifies how linkages can be created between various land uses and properties in and around commercial development, which will increase accessibility of vehicles, pedestrians, and cyclists and avoid creating additional burdens on traffic infrastructure for this major east-west arterial.

Planned IDOT Improvements

The existing right-of-way width along 159th Street between I-355 and Will-Cook Road is generally 50 feet to each side of the centerline for a total of 100 feet. The proposed right-of-way along 159th Street can generally be described as an acquisition of 20 feet to 25 feet to each side, but varies from a minimum of 10 feet and a maximum of 55 feet. The variation occurs along the Corridor due to the existence of the Forest Preserve, homes, structures, culverts, topography of the terrain and other existing roadside features or the proposed location of drainage detention. There are many temporary construction easements to re-grade driveways and entrances to meet the elevation of the expanded roadway.

The width of 159th Street will be expanded from one (1) 12 foot lane in each direction with no opposing lane separation to two (2) 12 foot lanes each direction separated by a 28 foot landscaped median. At intersections, one (1) or two (2) 12 foot left turn lanes will be carved out of the 28 foot median. Median breaks will only be allowed at signalized intersections and other regularly spaced intervals. Some breaks may not be regularly spaced but will vary depending on access points and roadways joining 159th Street.

Unprotected left turns across opposing traffic always account for a significant percentage of collisions on urban arterial highways. 159th Street has been designed by IDOT to reduce or eliminate left turns out of individual driveway entrances by proposing a barrier curbed median with right-in / right-out access.

Motorists will be required to turn right out of driveways/entrances along the corridor and continue to proposed full access intersection locations where U-turns will be permitted. Within the five mile project length there are 24 full access intersections at which motorists will be allowed to make U-turns (on average one full access location which U-turns are allowed every 1100 feet or 0.20 miles).

Within the limits of the project only four cross-roads are scheduled to be improved for any considerable distance north-south of 159th Street. Cedar Road, both north and south of 159th Street, and Bell Road, both north and south of 159th Street, will each be widened to two(2) 12 foot lanes in each direction separated by a barrier or stripped median. A portion of the median width will be used as a single or dual left turn lane(s) at the intersections. Parker Road will be widened to two (2) 12 foot lanes in each direction separated by a 12 foot left turn lane. Will-Cook Road improvements include adding a northeast-eastbound right turn lane. All other cross-road improvements include only improvements to the corner turning radii.

IDOT has attempted to address most of the issues along the 159th Street Corridor into the final design plans including minimizing the impact footprint to the greatest extent possible; maximizing access to businesses along the route; providing an underground utility corridor; providing or accommodating a beautification element (street lighting, village focal points, median landscaping) and providing a pedestrian/bike way element. The proposed improvements will create many benefits including increasing roadway capacity and lessening delays; increasing the movement of, goods, services and emergency vehicles; improving safety; and reducing current high accident rates.

Planned Will County Improvements

Will County has prepared a Phase I preliminary engineering report for a new SRA locally known as the Caton Farm – Bruce Road corridor. The proposed corridor will facilitate large volumes of east-west traffic over the Des Plaines River and will accommodate future population and employment growth, provide improved system linkages, accommodate future transportation demand, optimize modal interrelationships and increase travel safety. The new corridor, which remains unfunded for construction, will tie into 159th Street approximately 1/4 –mile west of Cedar Road. Currently efforts are underway by Will County to secure right-of-way along the corridor.

The existing 159th Street carries approximately 18,000 vehicles (according to IDOT as of June 2013) on the average day with slightly heavier volumes toward the eastern end. Roadway capacity of a two-lane roadway (one-lane each direction) is approximately 14,000 vehicles per day which means that 159th Street operates during AM & PM peak hours at a volume/capacity (v/c) ratio of 1.29. A v/c ratio in excess of 1.0 results in heavy congestion, delays, reduced air quality from idling vehicles and increases in crashes due in part to motorist impatience. Traffic volumes are projected to increase to 24,000-31,000 per day by the year 2040. Congestion, delays, air quality and safety are all projected to be adversely affected if no action is taken to improve and widen the existing roadway facility.

Proposed Improvements & Anticipated Benefits 159th Street from Gougar Road to Will-Cook Road

Proposed IDOT Improvement	Benefit
Limit Roadway Access to regular intervals	Reduces vehicle turning conflicts
Consolidate Driveway Access Points	Reduces vehicle turning conflicts
Prohibit left turns across centerline	Reduces vehicle turning conflicts
Provide turning lane bays	Removes vehicle from through lane while waiting to make turn
Provide increased right corner turn radii	Vehicle able to exit through lane quicker and more efficiently
Moderize Traffic Signals	Modern equipment sensors respond to vehicle presence
Interconnect Traffic Signals	Allows for progression of traffic though multiple intersections
Provide Barrier Median	Provides separation between opposing traffic
Provide Cross-Walks and Ped Signals	Increases safety of pedestrians
Provide designated multi-purpose paths	Reduces conflicts between cars and peds/bikes
Provide Protective Roadside Barriers	Reduces collisions with roadside fixed objects
Provide Outside Curb and Gutter	Reduces collisions with roadside fixed objects
Provide Signals "Left turn on Left Arrow Only"	Reduces Angle and Turning crashes at intersections
Provide Roadway Lighting	Increases night time visibility

Crash Rates

Crash rates along two segments and two intersections within the 159th Street Corridor, between Gougar Road and Will-Cook Road, approach those of the top 5% percentile in the State of Illinois for similar arterial roadways. Rear-end and turning type collisions make up over 75% of all crashes along the roadway. 159th Street accident rates and crash types are indicative of high volume, over capacity two-lane roadways with no shoulders.

It is estimated that the improvements will increase safety and reduce high accident rates by reducing rear end collisions by 60% by offering protected left turn/right turn bays; reducing left turn collision by 40% by including a barrier median thus prohibiting crossing over the centerline except at permitted intersections; and reducing head-on collision by 30% by including a barrier median to separate opposing traffic.



Improved Right-of-Way

Coordination with the Illinois Department of Transportation has been on-going during the development of contract design plans and specifications. A massive right-of-way acquisition is being undertaken, including 53.8 acres of right-of-way to widen the roadway and improve drainage. IDOT is currently in the process of purchasing property and securing easements along the roadway to accommodate the proposed improvements. The acquisition includes permanent and temporary easements for grading of drive-ways and for further drainage improvements.

Utility relocation planning is underway as well. Utility relocations have already begun and roadway construction is planned to follow. This schedule is dependent on right-of-way negotiations and acquisition of the 100+ parcels of property required for the improvements.

Roadway Access Points

Roadway access points along the 159th Street Corridor have been consolidated in the current IDOT plan. When completed there will be 24 access points (18 full access points and 6 full access signalized intersections) within the 5 mile project segment from Will-Cook Road to Homer Glen's western planning boundary at Gougar Road. Of this total, 17 access points (including 5 signalized intersections) will be located within Homer Glen's corporate boundaries at IDOT project completion. As can be seen in the table to the right, 19 of the 24 proposed access points are at least 1/4 mile interval from the adjacent roadway access point and 5 of 24 access points are spaced at approximately 1/8 mile intervals. The average access interval along the 5 mile project length is approximately 1140 feet or just less than 1/4 mile spacing. While 1/4 mile spacing is considered optimal, a distance of slightly under 1/4 mile is acceptable to maximize access to adjacent residential subdivisions, businesses and retail establishments.

Proposed Improvements for Roadways Crossing 159th Street

Station ³	Intersection	Left Turn Bay ¹		Signal		LOS ²	
		Existing	Proposed	Existing	Proposed	2010	2040
1200	Gougar Rd4	✓	✓	✓	✓	A	B
2450	Median Break		✓				
3775	Median Break		✓				
4900	Windmill Dr		✓				
5775	Median Break		✓				
6475	Leach Dr		✓				
7775	Median Break		✓				
9100	Cedar Rd	✓	✓	✓	✓	C	B
10425	Median Break		✓				
11625	Messenger/Hiller Dr	✓	✓				
13100	Median Break		✓				
14000	Median Break		✓				
14825	Stonebridge/Messenger Blvd	✓	✓				
15975	Median Break		✓				
17050	Parker Rd	✓	✓	✓	✓	D	B
18150	Median Break		✓				
19700	Hidden Valley Tr		✓				
20725	Annico Dr		✓				
21750	Oak Valley Tr	✓	✓				
22325	N. Bell Rd	✓	✓	✓	✓	E	B
23650	S. Bell Rd	✓	✓	✓	✓	D	B
25150	Median Break		✓				
25950	Crystal Creek Dr		✓				
26550	Twin Lakes Dr	✓	✓	✓	✓		
26880	Ace Hardware	✓					
27700	Will-Cook Rd	✓	✓	✓	✓	E	B

NOTES:

1) Currently left turns can be made across the centerline from any location. In the proposed condition, left-turns and u-turns across the centerline will be allowed only at full access location. A barrier median will block all other access across the centerline.

2) LOS = Level of Service for year noted during peak traffic hours for signalized intersections only.

3) Stations are in Feet. Distance between any two points can be calculated.

4) Gougar Rd was widened in 2008 during the construction of I-355 and is not part of this project.

Median Widening

The 159th Street Corridor Planning Study introduces many proposed improvements to the Corridor, but the addition of a central dividing median has evoked concern among many business owners located on the Corridor. Business owners have expressed concern that limited cross access around the median will decrease their "drive-by" consumer base.

It should be noted that landscaped medians are a widely accepted means to assist with roadway safety, traffic calming, and speed reduction efforts, in addition to their application in improving appearance. According to IDOT, adding a median to a road can reduce the crash rate about 37% and the injury rate about 48%. By directing vehicles to U-turns instead of executing direct left-turns, conflict points are reduced from 4 to 2.

The improvements will also address the discussed congestion issues. Increasing congestion on a roadway produces delays, frustrating consumers and leading to a negative image of the business environment. Well-managed arterials can operate at speeds well above poorly managed roadways, up to 15 to 20 miles per hour faster, meaning more traffic on the roadway and better exposure. The barrier median is intended to increase the capacity of the roadway by up to 30%, reduce vehicle speeds, and decrease motorist delays by up to 30%. The combined roadway improvements, including the median, will dramatically reduce traffic congestion that is projected to escalate on the 159th Street Corridor.

Although both the median and right-of-way additions will serve as positive features to the area, the Village recognizes the concerns of local business owners. Access is being changed from a previous condition that was familiar to business owners and their customers. However, the roadway design will likely not be significantly altered and IDOT will soon be proceeding with the plan's implementation. That said, the Village must take steps to alleviate concerns, facilitate a smooth transition, and offer assistance whenever possible. Potential actions for the Village may be offering businesses "We're Open" signage during the roadway's construction or promoting the Village's existing incentive program for updating commercial roadway signage. The Village should also consider creating alternative access through public side streets, service roads, or internal connections with neighboring developments that may help to increase accessibility on the median separated roads. Essentially, the Village should continue to play a proactive role in addressing business owners' concerns.

Primary Site Access

Access from the primary street should be minimized. For corner lots, side streets should be used to provide access to parking and drive-thru facilities. For mid-block lots, curb cuts should be shared with adjacent development, and cross-access should be encouraged. The Village should create alternative access through side streets, service roads, or internal connections with neighboring developments that will help to increase accessibility on the median separated roads. The Village should identify existing curb breaks to internal connectors, as well as create new internal connectors that will allow multiple businesses to benefit from one single curb access point or break in the median.

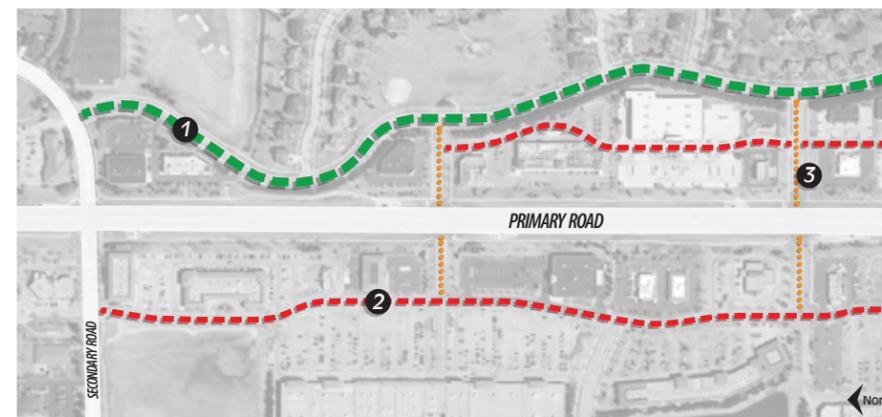
The Village should determine where a parallel collector, rear access, or frontage roadway is appropriate as an internal connection. Because site access will be less direct, the Village should consider directional signage on prominent street corners to direct customers to nearby business; directional signage is discussed in the **Regulatory Framework** section of this Plan in more detail. The Village should also emphasize full access from side streets and consolidated entrances, spaced far enough apart, to allow for dedicated turn lanes.

While the medians will require reconfiguring site access for businesses on 159th Street, site access from 159th Street will increase accessibility to small destination businesses or specialty stores, a substantial use on 159th Street. The businesses will benefit from access from lower speed roads, such as neighborhood collectors, as opposed to major congested roadways which may deter destination customers from making a trip. Reduction in access points may also spur the redevelopment, or re-classification, of commercial areas into cohesive shopping centers, with individual sites incorporated as outlet style developments.

Types of Internal Roadways

A frontage road is a local access right-of-way that runs parallel to a major roadway in order to eliminate the need for multiple access points. When sited along the back end of a parcel, this is known as a rear access road. Frontage and rear access roads are constructed as part of the development and subdivision process and are intended to serve as local access to development, thereby preserving the main roadway for longer-distance travel. Parallel collectors facilitate increased circulation on a major roadway like frontage and rear-access roadways, but act as a typical collector street. All three (3) types of access roads reduce congestion on the principal roadway.

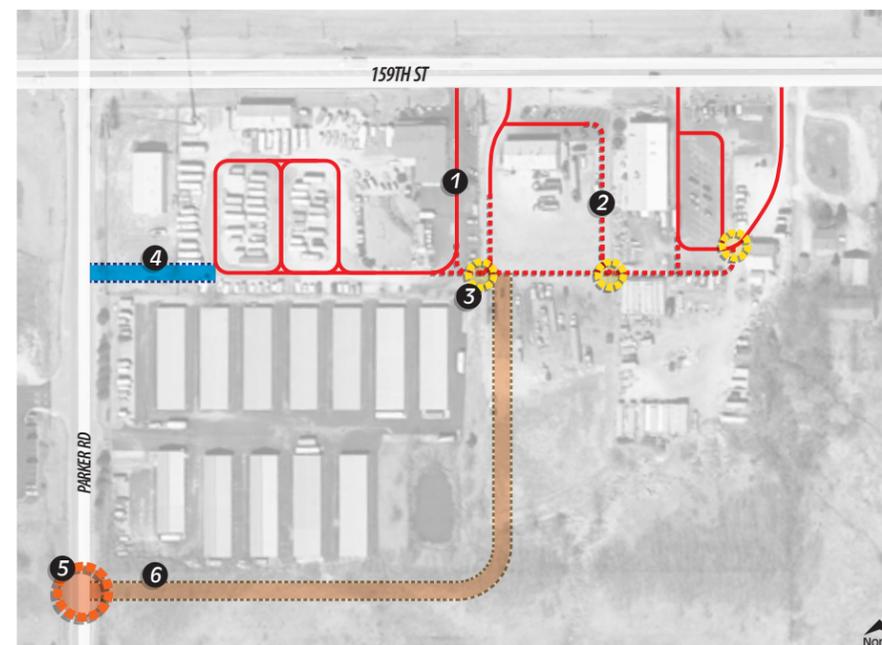
Cross access connections can also be made on existing parking lots to allow vehicles to travel from one site to another without having to get back onto 159th Street. As part of a successful access management plan, frontage roads, rear access roads, parallel collectors, and cross access connections should be developed as part of the overall transportation network.



Parallel Collector & Rear-Access Roads

This figure is an example of parallel collector and existing rear-access roads on Randall Road, a north-south arterial road in the western suburbs. The parallel collector, runs parallel to Randall Road and has distinct points of entry to commercial development as well as direct access to private residential drives. Rear access roads are provided to service the intense commercial developments, reducing the need for multiple curb cuts along the main road.

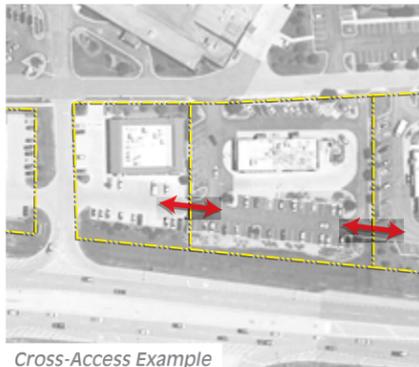
1) Parallel collector road, 2) rear-access road, and 3) key entry points



Site Improvement Rear Access Road

This figure illustrates potential site improvements to the area south east of the 159th Street and Parker Road intersection. These businesses currently utilize direct access from 159th Street, however, the IDOT proposed median will limit left-turning traffic access to Parker Road. The Village will ensure that these businesses are still adequately accessible by utilizing the vacant site south of the development for the construction of a rear-access route. While customers will still need to use Parker Road to access the site, the rear-access road will provide a safer and more efficient way of accessing the site.

1) Existing parking circulation pattern, 2) enhanced parking lot circulation, 3) cross access, 4) preferred alternative location for rear access roadway, 5) potential signalized intersection, 6) secondary alternative for proposed road through vacant lot and underutilized land



Cross-Access Example



Drive-thru Example



Existing Excessive Parking Area



Well Landscaped Parking Lot with Pedestrian Connections



Good Bike Parking Facility Example

Cross-Access

Cross-access, on-site vehicular connections that allow mobility between sites without using main thoroughfares, can have many public and private benefits. When adjacent lots have coordinated vehicle circulation, redundant curb cuts and driveways can be eliminated, thereby reducing conflict points with pedestrians and Corridor traffic. Businesses with cross-access benefit from their patrons' ability to easily visit multiple places per trip. Individual sites should have internal cross access drives to provide connections to full access points. Business should be encouraged to reconfigure parking lots to allow for cohesive cross access drives to connect properties. Businesses should also be encouraged to remove pedestrian obstructions such as fences and walls to allow for pedestrian connections between properties.

Drive-thrus

Although drive-thrus are not currently a use on the 159th Street Corridor, the large traffic volumes projected for the Corridor may create the potential for this type of development. Drive-thru facilities can create challenges for communities and property owners. For some businesses, however, a drive-thru may be an important amenity or necessity.

Whenever possible, drive-thrus should be located and designed to eliminate typical negative impacts. Drive-thru access and circulation should not impede pedestrian access from the public sidewalk or on-site parking lots. Drive-thru windows and canopies should not be visible from the public street and should integrate similar materials and architectural elements in the primary building. Circulation areas should be buffered from adjacent uses to minimize the impacts of noise and lighting.

Parking

The Village needs to address the excessive parking existing along 159th Street due to high traffic volumes and the type of development it will procure. Typically, retailers on corridors with high traffic volumes plan parking lot capacity for their busiest days of the year. These large parking lots visually detract from the aesthetics of commercial shopping areas but also create large areas of impervious pavement that increase stormwater runoff. Additionally, larger parking lots create a need for increased lighting, detracting from the Village's mission as an International Dark Sky Community.

Parking Capacity

To ensure functional parking areas do not detract from the character of the community, options should be given for lowering parking maximums and eliminating parking minimums. Although accommodating maximum capacity parking lots is a discouraged practice, it is understandable and may be a requirement for store location. In situations where accommodating large parking numbers cannot be avoided, the Village should encourage parking in the rear and sides of commercial buildings instead of just the front. This practice will serve to soften the appearance and prominence of parking on the Corridor.

The Village should also pursue creative uses for parking to reduce stalls and utilize lots more efficiently. Wherever possible, uses with different hours of operation should be eligible to share parking spaces at off-peak business hours, lessening the overall parking requirements needed to meet zoning compliance.

Shared parking facilities should be utilized between adjacent properties to minimize excess impervious pavement. Additionally, specific incentives, such as a reduction in vehicular parking if additional bicycle parking is provided, can also make more efficient use of development lots and encourage sustainable behaviors. Large parking areas may sometimes be unavoidable and should then be considered for a Park-n-Ride facility for residents travelling to either the Orland Park 153rd Street or Lockport Metra Stations. Park-n-ride facilities at commercial locations also directly link potential consumers with business at the times when they are picked up and dropped off.

Parking Lot Standards

To further ensure parking areas do not detract from the character of the Corridor and the overall community, the Village's parking design standards should be strictly enforced. The standards address stall sizes, aisle dimensions, appropriate parking configurations, and on-site circulation. They also address parking lot landscaping and screening so that, when visible from a public street, the lots do not detract from the aesthetic character of an area. Landscaped islands help delineate parking rows and drive aisles, as well as reduce the impervious surface area of the site. The Village should implement an On-Site Improvement Program to assist current property owners in upgrading their existing parking lots and installing on-site landscaping. The Village should also consider adopting amortization requirements as it relates to properties with non-conforming parking lots to encourage business owners to bring their properties up to code over an acceptable period of time.

Bicycle Parking

To foster the continued use of bicycles, amenities should be provided at popular destinations, local businesses, and community facilities. Local businesses and the Village should provide bike parking that is secure, highly visible, and convenient to busy attractions and establish bike paths that lead to shopping developments.



Parking Lot Bioswale



Permeable Pavers & Drainage Inlet

Stormwater Management Techniques

The Village should encourage native plantings, bioswales, and pervious pavers to improve stormwater management within private and off-street parking areas. Below are a few techniques effective at reducing stormwater runoff.

Bioswales

A vegetated swale is a broad, shallow channel with a dense stand of vegetation covering the side slopes and bottom of the channel. A vegetated swale is effective at reducing the velocity of flow through infiltration and preventing harmful pollutants from reaching water sources. While not a replacement to existing infrastructure, vegetated swales can help reduce the burden of stormwater on the Village stormwater sewer system during large storm events. Rain gardens are a popular application of vegetated swales. Specifics guidelines for vegetated swales and how to create a rain garden are included in the Village's Water Resources Best Management Practice (BMP) Guidelines.

Vegetated Covers

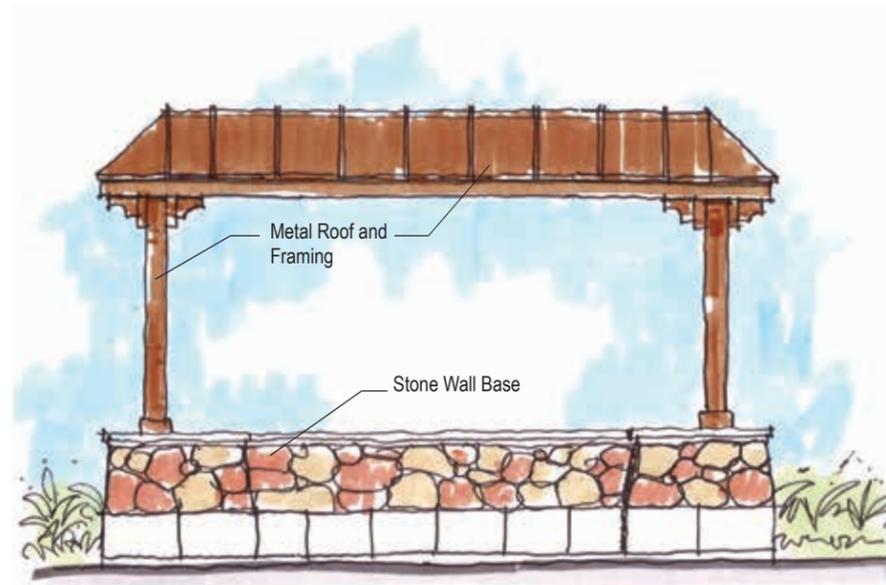
Vegetated covering is simply adding vegetation to stabilize and preserve disturbed land. Vegetative covering can be used to prevent erosion and sedimentation, increase infiltration, and dissipate the energy of hard rain. This technique can be applied at virtually any site and is a flexible technique to reduce the harmful effects of stormwater runoff.

Infiltration Trenches

Infiltration trenches are designed to filter pollutants as stormwater runs through the trench. Infiltration trenches are a good technique where land availability is limited. Infiltration trenches capture and treat small amounts of runoff, but are not effective for peak flows. This technique should be used in conjunction with other BMPs.

Pervious Materials

Pervious materials such as pervious pavers and porous concrete allow water to filtrate through pavement reducing runoff greatly in comparison to traditional parking lot materials.



Proposed Bus Stop Design

Public Transit

Public transit along the 159th Street Corridor consists only of Pace Bus Route 832, which operates every hour during the daytime in coordination with the Metra Commuter Rail service. Pace is the suburban bus division of the Regional Transit Authority, who manages the Chicago regional transit system. Although Homer Glen is not directly served by a Metra station, commuters have nearby rail access to downtown Chicago via the Southwest Service Line (SWS) at the Orland Park 153rd Street Station. The Orland Park Metra Station is served by Pace, however, there is no weekend or holiday bus service.

Pace Route 832 Joliet-Orland Square, begins in Joliet and runs north and east through Lockport, Homer Glen and Orland Park along 159th Street. Route 832 services the Orland Park 153rd St. Station, where passengers may transfer to the Metra into Chicago or other Pace routes. Route 832 continues east to serve Palos Community Care and concludes at Orland Square Mall in Orland Park. The route's daily ridership is approximately 31 riders (RTA, March 2013 count).

Bus stops along 159th Street are typically marked by a small freestanding sign. The stops lack a bus shelter and sidewalk connectivity.

The Village should promote the availability and use of Pace on the Corridor as a means of accessing commercial businesses, producing increased access to retailers, as well as providing transit to and from the nearby Metra stations. While bus ridership does not warrant near term action, attractive shelters should be installed, in the future, at designated stops where possible to provide waiting passengers with protection from inclement weather. Designated Pace bus stops (including, shelters, information kiosks, etc...) should be part of future streetscape improvements programs implemented within the Corridor. Furthermore, the proposed IDOT sidewalk and multi-use path network of sidewalks should link transit stops to the different areas of the Corridor. Paths should be safe, attractive, easily accessible and appropriately designed to accommodate a variety of travel modes.

As development occurs, the Village should work cooperatively with Pace to locate additional transit stops along the corridor as well as establish Park-n-Ride facilities. New bus stop locations should favor far-side stops along the Corridor to improve safety and minimize congestion, particularly for traffic making turning movements at intersections.



Existing 159th Street Right-Of-Way



Example of Connected Sidewalk Network

Pedestrian Mobility

Sidewalks are the foundation for pedestrian mobility and should provide a continuous connection throughout the Village. A complete sidewalk network improves pedestrian safety, enhances the environment, promotes healthy lifestyles, provides safe passage for pedestrians to and from activity centers, and improves transportation networks by allowing more people to access more places in more ways.

Pedestrian connectivity directly along the 159th Street Corridor remains absent, as the majority of traffic along this arterial is motorized and often lacks safety crosswalks, walk signals and sidewalks. In areas adjacent to the Corridor, most of the sidewalk connectivity is concentrated in residential subdivisions east of Cedar Road. However, not all residential areas are fully connected; west of Parker Road, residential sidewalk tracts are largely broken, often leading to undeveloped land or isolated lots as houses are built.

In 2010 the Illinois Department of Transportation incorporated "Complete Street" principles into the Bureau of Design and Environment Manual.

The new guidelines set forth a cost sharing arrangement between the Department/Local Agencies and policies regarding the justification, design, repair, replacement and construction of existing or new sidewalks, bicycle trails, pedestrian and bicycle structures, and collateral items required for these accommodations such as utility adjustments, signing, signals, pavement markings, lighting and right-of-way.

The 159th Street Corridor is being designed by IDOT using the Complete Streets principals. At the request of the Village, pedestrian and bicycle accommodations are being proposed at the major signalized intersections in the form of crosswalks, median refuge, and pedestrian pushbuttons/signal heads, and a multi-use path and sidewalk along the Corridor. Where these principals cannot be applied at this time, accommodations have been made into the design so that they can be incorporated into the Corridor at a later date. Examples of this include acquiring right-of-way in excess of that needed for the actual roadway improvement to reserve space that may be used in the future to accommodate benches, trash receptacles, wayward trail signing, lighting, utilities, landscaping, sidewalks and/or trail construction.



Multi-Use Path

Sidewalks/Multi-Use Paths

The existing 159th Street context is currently auto-oriented, but future IDOT improvements will change the Corridor dramatically. The IDOT improvements feature a designated multi-purpose path on the north side of the roadway that will extend from Gougar Road to Ravinia Ave (according to IDOT). The multi-use path provides the opportunity for pedestrian and bicycle travel to be a significant mode of transportation along 159th Street.

Efforts should focus on creating connections to the path by addressing three key components: 1) filling in gaps where there are no sidewalks, 2) replacing surfaces that are damaged or unsafe, and 3) ensuring that all segments meet accessibility requirements related to width, obstructions, etc. Where right-of-way widths are limited, connections to sidewalks will need to be provided on private property to create a continuous pedestrian network throughout the Corridor.

The goal is to provide safe and clearly demarcated connections across parking lots, driveways, and intersections. For example, sidewalks should continue through access drives into private properties creating a change in pavement materials. The change in pavement material helps to signal to drivers that the 159th Street Corridor is a pedestrian-friendly area.



Bike Trail

Trails

As well as creating connections to destinations along the 159th Street Corridor, the multi-use path also presents opportunities to connect the Corridor with the Heroes Trail System and the proposed Interstate 355 Veteran's Memorial Trail. The Heroes Trail System, which begins at West Beaver Lake Drive, links several subdivisions, parks, and other community features like Culver Park and Yangas Park. The Village will be utilizing a federal grant to extend the current Heroes Trail System south to 159th Street. The south extension of the trail will run north/south within the ComEd easement west of Bell Road from Martingale Lane to 159th Street. The south extension, which will be completed in 2015, will link several additional subdivisions into the Heroes Trail System.

Additionally, the Village is partnering with other public agencies, including the Forest Preserve District of Will County and the Illinois Department of Transportation to approve a multi-use regional trail system along the right of way of the highway extension of the Interstate 355 Veteran's Memorial Tollway. The trail will connect many miles of existing and future bike trails throughout Will, Cook and DuPage Counties.

The Village should emphasize the IDOT multi-use path as an extension of the Village's existing trail network. Actions to pursue include adding trail head signage and providing mileage signage along the path. Additionally, the Village should work with the Forest Preserve District to provide trailhead bike parking for travelers wishing to visit business and other destinations along the 159th Street Corridor.

Future Development Improvements

While the proposed IDOT improvements to the 159th Street Corridor reflect current development on the Corridor they do not reflect potential development. As development occurs, planned improvements such as the number of signalized intersection, median breaks, and turn lane locations may need to be increased or altered.

For instance, large scale businesses, like movie theaters or hotels, or agglomerations of business may introduce an increased volume of entrance and exit traffic at a specific intersection. In this instance, the Village should consider signalization if the uses meet the warranted activity levels. As development occurs and more people visit the area, the Village must be attentive to the future transportation needs of the Corridor.

Chapter 7

Infrastructure & Utilities Plan

Infrastructure and utilities are an important consideration for the 159th Street Corridor's development. Currently, much of the Corridor is undeveloped, and many opportunity sites are not serviced by utilities such as water, sewer, and electricity which are essential for new development. To accommodate the desired vision for the Corridor, businesses will need municipal utilities and the Village will need to expand utility services to these desired growth areas.

The **Infrastructure & Utilities Plan** establishes a coordinated vision for Homer Glen's infrastructure and utilities for the 159th Street Corridor. This Plan considers ways to tailor infrastructure and services to suit the needs of the 159th Street Corridor and its anticipated long-term development patterns. Additionally, the goals, objectives, strategies, and recommendations of this section are closely coordinated with other investments related to land use and development, transportation, parks and open space, and the design.

Electricity

Power in Homer Glen is provided by Commonwealth Edison (ComEd). ComEd has electrical lines in easements within the existing public right-of-way on one or both sides of 159th Street along the entire length of the project area between Interstate 355 and Will-Cook Road. Various service/feeder lines also exist parallel and across the roadway. While some buried lines do exist, the vast majority of ComEd facilities are suspended overhead and pole mounted.

Two major ComEd High Voltage Power Transmission corridors cross 159th Street within the project limits. One crossing runs north-south across 159th Street approximately ¼-mile west of N. Bell Road. The other runs in a SE to NW alignment and crosses 159th Street approximately ¼-mile west of Will-Cook Road. While none of the High Voltage Transmission lines will be affected by the proposed widening of 159th Street by IDOT, it is expected that ComEd relocation will be a major effort. Costs to relocate the ComEd lines will be the responsibility of ComEd. Costs to upgrade from future aerial pole suspended lines to buried lines are normally funded by the requesting agency, in this case, the Village of Homer Glen if it so chooses, and the Homer Township Highway Department, for the portion of the Corridor west of Cedar.

Comcast & AT&T

Comcast and AT&T provide Cable TV, Phone and Internet service throughout the 159th Street Corridor. Generally these lines are run on ComEd poles by agreement within the existing public right-of-way on one or both sides of 159th Street along the entire length of the project area between Interstate 355 and Will-Cook Road. Various service/feeder lines also exist parallel and across the roadway. While some buried lines do exist, the vast majority of Comcast and AT&T facilities are suspended overhead and pole mounted. It is expected that Comcast relocation will be a major effort. Costs to relocate the Comcast and AT&T lines will be the responsibility of Comcast or AT&T. Costs to upgrade from future aerial pole suspended lines to buried lines are normally funded by the requesting agency, in this case, the Village of Homer Glen, and the Homer Township Highway Department, for the portion of the Corridor west of Cedar Road.

Recommendation

Along 159th Street power lines, which vary in shape, angle and placement, are highly visible to motorists and pedestrians, providing an unsightly appearance along the Corridor. These poles create a long linear eyesore to residents and travelers on the Corridor as well as diminish the Village's goal to be compatible with nature. Additionally, the presence of overhead utilities may also make parkway landscaping, unique street lights, and street banners difficult to implement along the Corridor.

The Village of Homer Glen should work with property owners, utility companies, and developers to bury the utility lines or relocate them to the rear of future developments. These options are costly but to beautify the corridor and facilitate streetscape improvements and parkway trees, their relocation or burial should be a priority. The Village should also require developers and utility companies to screen utility boxes from the street either through landscaping, fencing, or both.

Cost to Bury Existing Overhead Utility Lines

Burying utility lines within urban and suburban roadway right-of-ways is expensive and difficult due to the undesirability of placing utilities under pavements and congestion of available space. Coordination with IDOT and utility companies to bury the relocated lines instead of placing them overhead on poles may be accomplished along 159th Street. It can be expected that ComEd will not bury the lines unless an agreement is in place with the local municipality and the Highway Department to fund the difference between the base cost and desired work effort.





Gas & Oil Transmission

Nicor Gas

Nicor Gas (Nicor) has natural gas lines in easements within the existing public right-of-way on one or both sides of 159th Street along the entire length of the project area between Interstate 355 and Will-Cook Road. Various service/feeder lines also exist parallel and across the roadway. All of the lines are buried except for gas valve regulators which exist above ground in several locations. It is expected that Nicor's relocation will be a major effort. Costs to relocate the Nicor lines will be the responsibility of Nicor.

Recommendation

In addition to above ground Nicor gas valve regulators, there are also many above ground utility boxes located throughout the 159th Street Corridor. Although these boxes are a necessity, specific placement of utility boxes is often left up to the judgment of the utility at the time of construction. The Village should coordinate with utility providers and jurisdictional agencies in the future to carefully place utility boxes, consolidate boxes where possible, and standardize materials and color in order to minimize the visual impact of the utility boxes, whenever utilities are modified or upgraded.

Pipelines

There are six (6) oil or gas transmission pipelines which cross the 159th Street in the project area. One-half mile east of Gougar Road crossing 159th Street in a perpendicular angle are petroleum pipelines owned by Wolverine Pipeline Company and Exxon Mobil. They run parallel to each other within a 50-foot easement. Approximately one-half mile west of Cedar Road is another perpendicular crossing of two gas pipelines owned by BP America and TEPPCCO. They run parallel to each other within a 75 foot easement. Approximately 3/8th mile east of Cedar Road running under 159th Street at a 45 degree angle within a 50 foot easement are two oil pipelines owned by Unocal and Chicap. It is not anticipated that any of the pipelines will be impacted by the future widening of 159th Street. It is anticipated that each of the pipelines will require protection during construction which may include protective concrete encasement.

Illinois-American

The 159th Street Corridor is currently served by three different water systems. The only system of water for many along the route are private wells which are regulated and monitored by Will County Department of Public Health and serve many of the Corridor businesses and older homes abutting the Corridor. The other water system along the Corridor providing service is Illinois American Water Company and the Lockport FPA.

The majority of the 159th Street corridor is within the water planning area of Illinois American Water Company. The area between Hiller Drive/Messenger Woods and Gougar Road is within the Lockport Bonnie Brea Facilities Planning Area (FPA). The Glenview Walk subdivision will eventually connect to the Lockport Bonnie Brea FPA when the Fiddymet Creek sanitary sewer is constructed and the eastern limits of the FPA will be re-defined. Water Extension of water systems will be driven by development and funded by the Village of Homer Glen. Recoupment of the capital expenditures to design and construct water systems will likely come from recapture fees placed on owners/developers/users.

Sanitary Waste Collection & Treatment

The 159th Street Corridor is currently served by five different wastewater collection/treatment systems. The only system of treatment for many along the route are private septic systems which are regulated and monitored by the Will County Department of Public Health. Other sanitary systems along the Corridor providing service include the City of Lockport - Bonnie Brae Waste Water Treatment Plant, City of Lockport - Lockport Heights Waste Water Treatment Plant and Illinois American Waste Water Treatment Plant near 159th/South Bell Road.

The 159th Street Corridor is within the wastewater planning area of either the City of Lockport or Illinois American Water. Currently there are no plans to provide sanitary sewer extensions or service, however, the treatment plants noted above have been constructed to accommodate future expansion as warranted and funded. The proposed Fiddymet Creek sanitary sewer has been through the preliminary feasibility and engineering process. Final design plans are nearly complete although not approved by the Village of Homer Glen for construction. Extension of Sanitary Waste Collection systems will be driven by development and funded by the Village of Homer Glen. Recoupment of the capital expenditures to design and construct waste collection systems will likely come from recapture fees placed on owners/developers/users.

Utility Extension

The Village of Homer Glen has expressed an interest in expanding water and wastewater services within and around the 159th Street Corridor to accommodate future development. Currently, there are hundreds of acres, including land located beyond the current corporate limits within the Village's western planning jurisdiction, that are not serviced with water or sewer mains. Extending water and sewer mains along the entire length of the Corridor into the Village's planning jurisdiction near Interstate 355 is necessary for these properties to achieve their highest and best use potential.

The Village should ensure thoughtful expansion of utilities by developing a program that identifies priority locations for water and sewer infrastructure extension along the Corridor. This may include coordination with developers to identify sites for utility expansion and communication with private facility providers.



Stormwater Management

Recent storm events have placed heightened scrutiny on the importance of stormwater management in the Village. The IDOT roadway improvements are expected to foster additional development along the Corridor that will add to the amount of impervious surfaces. IDOT has planned a number of improvements to mitigate the effects of excess stormwater runoff including replacing the existing open drainage system of gravel shoulders draining to roadside ditches and then to outfalls with a closed drainage system. The closed drainage system will drain the pavement using inlets and/or catch basins in the curb line to convey flow from storm sewer pipes to detention areas and eventually to outfalls.

There are no known areas of pavement flooding along 159th Street within the project limits. There are no known problematic stormwater areas. IDOT has plans to provide storm sewer piped roadside detention areas in many locations along the roadway. These detention areas may be combined with limited in-pipe storage and are generally required to compensate for the increased pavement area being proposed.

Stormwater detention for the proposed roadway expansion has been provided within the proposed 159th Street right-of-way at sixteen locations along 159th Street between Gougar Road and Will-Cook Road. The design of these detention systems has neither been coordinated with adjacent property owners nor oversized to accommodate future development along the route.

Additionally, the IDOT improvements for 159th Street accommodate stormwater into their road design by acquiring additional right-of-way for grass-lined swales along the roadway. Landscaping in the median and along the proposed pedestrian paths will provide further infiltration features. Overall, the new improvements are projected to mitigate stormwater on 4 acres of local wetlands and have a zero difference impact on the Marley Creek, Fiddymont Creek, and Spring Creek floodplains.

The Village has also engaged in significant efforts to mitigate the effect of stormwater on its residents through their Water Resource Management Ordinance and the Water Resource Best Management Practices (BMP) Guidelines. The ordinance and guidelines provide the Village with a guide for implementing stormwater best management practices when considering new development. The ordinance and the BMP Guidelines emphasize the use of bioretention management practices to minimize adverse water quality and large water volumes on property within the Village. The Village's Water Resource Management Ordinance and Water Resources BMP Guidelines have prepared the Village to manage potential new development and integrate stormwater design into new development plans.

Recommendation

The Village should utilize its Water Resource Management Ordinance and Water Resources BMP Guidelines when reviewing development proposals for the 159th Street Corridor. This includes encouraging the use of native plantings, swales, and pervious pavers to improve stormwater management within private off-street parking areas. These techniques are effective stormwater management practices and generally less expensive to install and maintain than traditional curb-and-gutter.

The Village should parallel IDOT stormwater improvements along the Corridor with improvements along new and existing entranceways and side streets. To encourage stormwater management on private property, the Village should consider Village-sponsored financial incentives (i.e. matching funds, grants, low interest loans, permit rebates, etc.) to assist local property owners interested in making on-site stormwater improvements to their properties.

The Village should also minimize the number of stormwater basins along the corridor to maximize developable space. This can be done by coordinating with adjacent property owners to encourage shared stormwater facility locations along common property lines to reduce infrastructure costs and improve stormwater efficiencies.

Chapter 8

Economic Development & Implementation Plan

Completion of the Village of Homer Glen 159th Street Corridor Planning Study is, in many ways, the first step in the process of improving the Corridor, not the last. The 159th Street Corridor Planning Study is the result of considerable effort on the part of the Village of Homer Glen, the Corridor Planning Advisory Committee (CPAC), Village residents and stakeholders, Village staff, Village Board, and the Plan Commission. The final Corridor Plan represents the consensus of all involved.

The Village has committed to the successful implementation of this Plan to establish a land use and growth strategy for the Corridor, support economic development, make the necessary transportation and infrastructure improvements, and foster intergovernmental coordination and cooperation. The Village must have a significant role in the implementation of this Plan due to the size of the 159th Street Corridor area (approximately 5 miles in length), its regional significance, the multiple property owners involved, and the diversity of planning opportunities and issues within the study area.

To effectively implement the recommendations presented in the 159th Street Corridor Planning Study, a number of implementation tools should be used by the Village. This chapter highlights the implementation tools available for the Village to work toward fulfilling the recommendations of the Plan.

Strategies, actions and policies to implement the Plan are described on the following pages, organized in the categories listed below:

- Administrative Actions
- Regulatory Actions
- Capital Improvements
- Economic Development
- Review and Update Actions
- Potential Funding Sources

Administrative Actions

The following components require administrative action and/or public policy to implement. These actions do not require a significant allocation of funds and can be completed in a relatively short time frame compared to other recommendations.

Plan Related

Adopt the Plan. The 159th Street Corridor Planning Study should become the Village of Homer Glen's official policy guide for improvement and development for the 159th Street Corridor. It is essential that the Plan be adopted by Village Board for use by the Village Board, Village staff, and commissions to review and evaluate all proposals for improvement and development within the study area.

Use the Plan. The Plan has been designed with a great deal of emphasis on the use of graphics with the intent of more easily illustrating specific ideas and recommendations. The purpose of this graphic approach is to help ensure that the Plan is easily understood.

Development Related

Ensure Development Plans work with the Plan. Review plans and proposals from the development community and utility companies against the recommendations and framework plans of this document. Whether new tenant build-out, facade renovation or full redevelopment, the 159th Street Corridor Planning Study should be used in communicating the Village's vision for the Corridor to business owners and investors.

Communication Related

Meet with residents, businesses and key property owners. Meet with residents, businesses, key property owners, institutions and others to review the recommendations of the Plan and discuss future improvements to begin to work together for a shared future. Consider reaching out to owners of identified catalyst sites to ensure they are aware that their parcels are not "targeted" for redevelopment, but considered key to sustaining the Village's healthy neighborhoods should they be sold or redeveloped.

Make the Plan available. The Village should post a copy of the Plan on the Village's website for download. A hardcopy of the Plan should also be kept at the Homer Glen Village Hall for review and the Homer Township Public Library.

Promote Communication and Cooperation. The Village of Homer Glen should assume the leadership role in implementing the 159th Street Corridor Planning Study's recommendations. In addition to carrying out the administrative actions and many of the public improvement projects recommended in the Plan, the Village may choose to administer a variety of programs available to local residents, businesses and property owners. In order for the 159th Street Corridor Planning Study to be a success, it must be based on a strong partnership between the Village, other public agencies, utility companies, IDOT, various neighborhood groups, local business owners, and residents.

Regulatory Actions

Adoption of the 159th Street Corridor Planning Study should be followed by a review and update of the Village's current development controls including zoning, subdivision regulations, and other related codes and ordinances such as the sign ordinance. It is essential that all development controls are consistent with and complement the 159th Street Corridor Planning Study.

The 159th Street Corridor Planning Study sets forth policies regarding the use of land within the commercial areas and establishes policies for the quality, character, and intensity of new development and redevelopment. The Plan's policies and guidelines should greatly assist the Village in amending zoning and development code regulations that can better reflect the needs and aspirations of the community for this important area of the Village.

Update the Village's Comprehensive Plan and Future Land Use Map. The Village should adopt the 159th Street Corridor Planning Study as an update to the Village's Comprehensive Plan to reflect the new land use designations so the Future Land Use Map is consistent with this Plan.

Update the Village's Zoning Regulations. Zoning is one of the most powerful tools municipalities have to control, guide, and regulate land use and development. In addition to designating what uses are permitted in what locations, zoning also sets the standards for the physical form and certain aesthetic components of the built environment – height, setbacks, lot coverage, floor area, parking requirements, signage, landscaping, and more.

Together with the Land Use Plan, the Village's Zoning Regulations need to reflect acceptable recommendations in the 159th Street Corridor Planning Study. Zoning regulations should encourage redevelopment and enhancement of properties, while improving the image and character of the Corridor.

The Village should consider Form Based Codes to better regulate the built form of the Corridor. Form Based Codes are regulations, not guidelines. Form-based codes address the relationship between building facades and the public realm, the form and mass of buildings in relation to one another, and the scale and types of streets and blocks. The regulations and standards in form-based codes are presented in both text and clearly drawn illustrations.

Eliminate Non-Conforming Signs and Sign Copy. Consideration should be given to adopting amortization requirements as it relates to non-conforming signage, in order to remove billboards and other undesirable and non-conforming signage from the Corridor, over an acceptable period of time. Additionally, consideration should be given to amending sign regulations for outgoing tenants and owners. If there is a business closure, business names should be removed from the premises. In the case of interchangeable panels, a blank black or white panel should be inserted.



Amend Regulations for Landscape Screening, Walls, and Fences. A key amendment could dramatically improve both the commercial and residential character of the Corridor. Consideration should be given to amending the development regulations to require landscape screening and/or berming to screen parking areas adjacent to the front property line. Required screens should take into consideration the need to allow the visibility of business frontages and signage. This would dramatically improve the appearance of the Corridor in areas where surface parking lots are located along the front of the property and adjacent to the sidewalk and street.

Promote the streetscape and beautification improvements recommended in this Plan. This Plan identifies several improvements aimed at beautifying and improving the streetscape throughout the Corridor. In addition, the Design Guidelines of the Plan provide additional guidelines and recommendations to enhance the built form.

Update Transportation Policies. Consider the development of access control policies and standards to be integrated into updated Zoning Regulations. The policies should reduce the number of individual driveways along the Corridor in favor of shared access, and promote healthy walkable neighborhoods.

Improve Pedestrian Environments, Landscaping, and Lighting On-Site. Several ordinance amendments could address smaller, site specific components and improve the appearance of the Corridor incrementally as development occurs over time. The development regulations should be amended to require clear on-site pedestrian sidewalk connections from the public walk to the front walk/building of the business located on the site. Wherever possible, the route should not cross drive aisles and parking lots. This simple code revision could dramatically improve the pedestrian safety and environment for commercial properties. Both landscaping and lighting requirements for development sites could be improved by amending the ordinance to require appropriate landscape improvements on private property to coordinate with public streetscape improvements, such as a particular plant palette and for larger sites, planters, irrigation, and a maintenance plan. Also, consideration should be given to amending regulations to require decorative building and site lighting as a requirement for new construction, redevelopment, and exterior renovations. The lighting would need to be compliant with the requirements of the Village's Outdoor Lighting Ordinance.

Corridor Zoning

Related to the planning framework are the ordinances and codes that regulate the built form and uses along 159th Street. This is an important distinction, since in order to effectively implement the Plan, the Village's Zoning Ordinance, and map, must be able to accommodate and foster the development desired by the community. To this end the 159th Street Corridor Plan provides an analysis of the Village's Zoning Ordinance, and presents recommendations to assist the community in realizing the overall identity and character of the 159th Street Corridor.

Existing land use along the 159th Street Corridor varies significantly throughout the Corridor, and so does the zoning. The Village's Zoning Ordinance and 2013 Zoning Map identifies 20 zoning districts, 15 of which front the 159th Street Corridor, including both non-residential and residential districts. The varying zoning districts, and perhaps most importantly, disconnects with the Comprehensive Plan, fail to provide a regulatory framework that will foster development desired by the community.

Disconnection between Plans, Use & Zoning

The varying zoning districts and disconnects with the Comprehensive Plan do not provide a regulatory framework that will create the type of development desired for the 159th Street Corridor. This is problematic in that it may be deterring investment along the Corridor because zoning consistent with the Comprehensive Plan is not in place; for example, parcels on the south side of 159th Street, near Bell Road are currently zoned rural residential but the Village's Comprehensive Plan envisions commercial development for this land. The Village is currently working with property owners to rezone parcels along 159th Street to appropriate commercial zoning. Several of these rezonings have occurred over the last year and this process should continue.

To create a more efficient rezoning process, the Village should prioritize parcels that are not compliant with the Comprehensive Plan for rezoning based on their development opportunity. This may include parcels located near major intersections or in already established commercial developments. Additionally, the Village of Homer Glen should review and update the Village's various development controls that apply to the Corridor including portions of the zoning ordinance and other related codes and ordinances.

Signage

Signs not only communicate the nature of individual businesses, but also influence the overall image and character of Homer Glen. In the Village, parameters of signage are dictated by the Village's Zoning Ordinance. According to the Ordinance, each business in a commercial district is permitted to have one ground-mounted sign up to 10 feet in height predominantly constructed of masonry or stone. Prohibited signage include traveling message signs, message center signs, intensely or directly illuminated signs, uncovered neon signs and window signs covering more than 30% of the window front. The Village requires wall signs made of individually constructed letters, rather than large box or tray signs. In addition, in the case of preexistence of the ordinance, nonconforming signs are deemed to be legally nonconforming.

Currently, many of the existing signs along 159th Street were constructed prior to the Village's regulations and are excessive, lacking in character, and contribute to the overall cluttered appearance of the corridor. Overtime, new commercial signs will be constructed that reflect the standards in the Village's Zoning Ordinance or be replaced by property owners taking advantage of the Village's Non-Conforming Sign Replacement Loan Program.

The Plan recommends that permanent business signage should be lower-profile monument signage in order to reduce clutter and improve navigation. To create a more cohesive signage along the Corridor, the size, material, color, and shape of commercial signage should complement the architectural style and scale of the building. Further design guidelines for commercial signage are outlined in **Chapter 5 Design Guidelines** of this Plan.

Current Zoning



Legend

Agriculture

- A-1 Agriculture
- A-2 Rural Residential

Residential

- E-2 Single Family Rural Residential
- R-1 Single Family Residential
- R-2 Single Family Residential
- R-2A Single Family Residential
- R-3 Single Family Residential
- R-3A Single Family Residential
- R-4 Single Family Residential
- R-5 Single Family Residential
- R-6 Attached Single Family/Multi-Family Residential

Commercial

- C-1 Neighborhood Commercial
- C-2 Local Business
- C-3 General Business
- C-4 Highway Commercial
- C-5 Office & Research Park
- C-6 Commercial Recreational

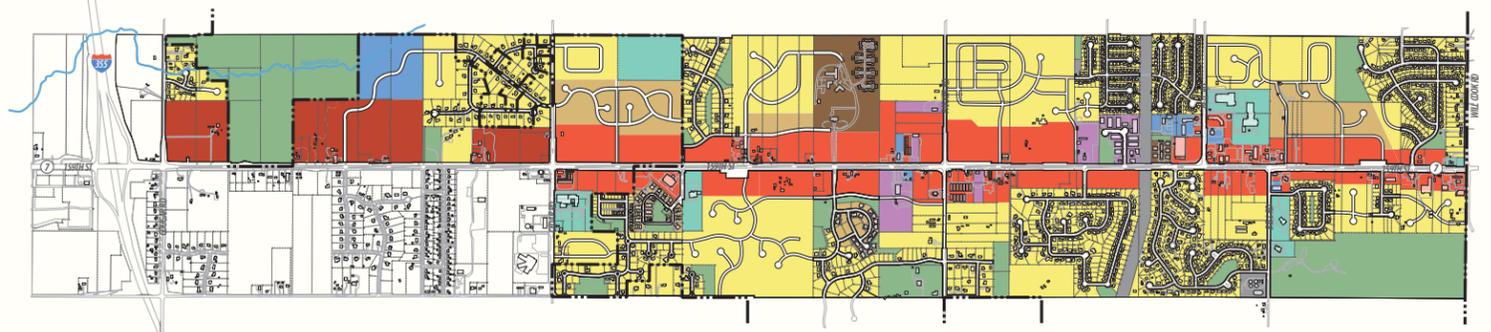
Industrial

- I-1 Industrial
- I-2 General Industrial

Zoning & Land Use Compatibility

The Village's Current Zoning districts and map will need to be updated to better reflect the land use recommendations made in the 159th Street Corridor Planning Study.

159th Street Corridor Study Land Use Plan



Legend

- Homer Glen Village Limits
- Single Family
- Duplex/Townhomes
- Multi-Family
- Institutional
- Regional Commercial
- Local Commercial
- Office
- Industrial
- Open Space
- Utility

SEE PAGE #18 FOR MORE DETAIL

Billboards

Several billboards are located along 159th Street that may present a hindrance to establishing the desired community aesthetics and visual character of the corridor. Some of the billboards are situated within unincorporated areas governed by Will County, which does not prohibit billboards. Other billboards are located within the Village of Homer Glen and are legally non-conforming, as they were established prior to the Village's off-premise sign prohibition.

The Village should develop and enforce a signage ordinance to promote appropriate and aesthetic signage in the Corridor, including the amortization of non-compliant signage to remove billboards and other undesired signage along 159th Street over an acceptable period of time. The Village should also continue to review and approve signage as part of the site plan review process to ensure that future signage reflects the desired character of the Corridor and the overall community.

Directional Signage

In an effort to offset any adverse effects the roadway improvements will have on existing business, the Village may consider allowing directional signage. Directional signage will grant businesses whose access has been reconfigured, or minimized to one major access point, by allowing commercial wayfinding signage at primary access points.

Outdoor Lighting Ordinance

In 2007, the Village of Homer Glen adopted the outdoor lighting ordinance, a stand-alone ordinance to address the issue of light pollution in urban areas. The Village ordinance presents a clear set of guidelines for outdoor lighting aimed to preserve the ability to view the night sky while also reducing the glare of light for night travelers and financial and energy waste due to indiscriminate illumination (Homer Glen Outdoor Lighting Ordinance). The code outlines requirements that address light direction and control, light intensity, gross emissions of lights, light trespassing, and permitted hours for outdoor lighting. Lighting requirements differ by residential, commercial, agricultural, and industrial land use because of the varying application of light on these types of properties. The Village gained International Dark Sky Community (IDSC) designation due to its dedication to preserving the night sky through the ordinance.

The 159th Street Corridor Planning Study has prompted concern that development of the corridor, including road improvements, will be detrimental to the night sky. However, conformity to the new lighting ordinance considerably limits light pollution. The Village has already successfully approved new code-compliant developments, including Eagle Rock Community Church, Midland Federal Savings and Loan, Firestone Complete Auto Care, First Community Bank, and Silver Cross Health Center. This new development serves as an indication that existing regulations can accommodate new development.

To ensure new development along the 159th Street Corridor does not add excessive light pollution to the Village, the Village should utilize the lighting requirements outlined in the Outdoor Lighting Ordinance when reviewing development proposals.

Capital Improvements

The Village should continue to maintain its Capital Improvements Plan (CIP) to plan and budget for future actions as outlined in this Plan. The Village's Capital Improvement Plan identifies major public construction projects and improvements. Capital projects are permanent physical or system improvements of at least \$50,000 with a useful life of at least 10 years; or a study of at least \$50,000 that will lead to such projects. Examples of projects include bridges, public restrooms, and sewer main replacements. This Plan identifies many recommendations that should be integrated into the Village's CIP. The Village of Homer Glen's financial resources will always be limited and public dollars must be spent wisely. The CIP would allow the Village to provide the most desirable public improvements, yet stay within budget constraints. The following is a list of potential, major capital improvements identified in the Plan.

Transportation & Circulation

Cross Access. The Village should begin to work with private landowners to create improved cross access between parcels, as identified in the Plan. Identifying willing volunteers to be aided with some public funding is a good way to implement a prototype in the Corridor to be followed by others.

Pedestrian and Bicycle Improvements. Work to secure funds for additional sidewalk connections, bike route connections and other pedestrian amenities as identified in the Plan.

Transit. The Village should encourage the use of transit by installing busbays, bus stops, and crosswalks at major development sites as development occurs along the Corridor.

Streetscape & Beautification

Streetscape Improvements. Work to improve the appearance of the Corridor including improved landscaping, lighting, and gateway signage as indicated in the 159th Street Corridor Planning Study. Improvements along the Corridor will require coordination with IDOT.

Public Signage Improvements. Design and implement new additions or improvements to street signs and wayfinding signage, especially at identified intersections to provide for better navigation throughout the Corridor.

Utilities

Burying Overhead Utility Lines. The Village should continue to work within its current program but also in coordination with utility companies to bury overhead utility lines. The Village should also work with utility companies on the placement of future utilities to ensure that they are located in areas that do not deter from the desired appearance and character outlined in the Corridor Study. Additionally, uniform painting of highly visible transformers, switch boxes and the like should be coordinated with the appropriate utilities and agencies.

Utility Extension. The Village should ensure thoughtful expansion of utilities by developing a program that identifies priority locations for water and sewer infrastructure extension along the Corridor. This may include coordination with developers to identify sites for utility expansion and communication with private facility providers.





Economic Development

New development and redevelopment within the Corridor will provide Homer Glen with the opportunity of enhanced revenue sources. Throughout the planning process, the community has expressed the desire to redevelop and improve the appearance of the Corridor. Given the importance of the retail, service, and office uses located within the Study Area, it is important that the Village continue to support existing businesses, while actively pursuing new businesses to locate within the Study Area. Economic development strategies consistent with the recommendations of this Plan include:

- **Marketing.** Hold regular meetings with the business, real estate, and development communities to apprise them of active changes and improvements in the Village. Providing up-to-date site inventories on properties available for development and continued participation in the Homer Township Chamber of Commerce can be useful in making business contacts.

- **Business Retention.** Along with promoting new development, the retention of existing businesses should be a priority. The Village is aware of the importance of maintaining contact with retail and employment businesses in the community to stay informed of business needs. Much of the Village's future economic development will result from improved performance of local retailers and manufacturers, as well as the expansion of these businesses in the community. Continued support of and cooperation with the Homer Township Chamber of Commerce, and other business related entities, is recommended to maintain these efforts.

- **Business Community Involvement.** It will be important to continue to build a strong relationship with the business community. The Village makes a high priority of working with the development community, businesses and landowners to realize economic change and physical improvement, as recommended under this Corridor Study. Many recommendations of the Corridor Study involve business interests, and strong partnerships between the Village and business community will greatly help to facilitate success in these efforts in the future.

Review & Update Actions

In order for the Plan to remain as up-to-date as possible, the document needs to be used and reviewed on a regular basis. The 159th Street Corridor Planning Study is not a static document and as changes in the community and along the Corridor occur, especially new or different ideas, implemented capital projects, they should be part of the review and update process. These changes can be brought forth to the Village at any time, and should be part of the 159th Street Corridor Planning Study's review process. The following procedures should be used in reviewing and updating the 159th Street Corridor Planning Study.

Day-to-Day Monitoring & Administration

To ensure that the Village is not alone in the monitoring and review of the Corridor Study it is important for the document to be made available to the public, the Homer Township Chamber of Commerce, community organizations, and other agencies. The Village Board is responsible for setting the policies that will promote the goals and objectives of the 159th Street Corridor Study, however, Village Staff, who is responsible for the Village's planning and zoning, economic development, and engineering, is the most appropriate to carry out the day-to-day plan administration.

Responsibilities would include:

- Day-to-day administration and interpretation of the Corridor Planning Study
- Maintain a list of future amendments, issues or needs which may be added, changed, or removed from the Corridor Planning Study
- Undertake any additional studies recommended in the Corridor Planning Study
- Recommend policy revisions and changes to the Village Board.

Plan Review & Progress Report

Although a proposal to amend the Plan can be brought forth by recommendation at any time, the Village should regularly undertake a systematic review of the 159th Street Corridor Planning Study. The Village should initiate a full review of the Corridor Study every 5 years. In addition, each year, in conjunction with the development and approval of an annual workplan, the Village should monitor accomplishments, successes, outstanding steps, and new issues or opportunities. Preparation of an annual workplan should coincide with the preparation of the annual budget and capital improvement program. Routine examination of the Corridor Planning Study will help to ensure that the document remains relevant and on the forefront.

Strategies, Incentives & Funding Sources

In a financially constrained environment it is vital that the Village make the most efficient use of its current resources and identify alternative sources of funding for new programs that cannot be readily accommodated within the existing budget. The following discussion outlines funding sources and tools that the Village could utilize in its efforts to attract development. These funding sources could be used to support various incentives and provide a general source of funding for capital improvements.

Local Funds & Sources

Tax Increment Finance (TIF)

TIF utilizes future property tax revenues generated within a designated area or district to pay for necessary improvements and incentivize further reinvestment. As the Equalized Assessed Value (EAV) of properties within a TIF District increases, the incremental growth in property tax over the base year that the TIF was established is reinvested in the area. Local officials may also issue bonds or undertake other financial obligations based on the growth in new tax revenue within the district.

The maximum life of a TIF district in the State of Illinois is 23 years although the TIF district can be extended via approval from the Illinois state legislature. Over the life of a TIF district, the taxing bodies present within the district, such as school or park districts,

receive the same amount of tax revenue that was generated in the base year in which the TIF was established. There are provisions that allow for schools to receive additional revenue. TIF funds can typically be used for infrastructure, public improvements, land assemblage and in offsetting the cost of development.

A study must be undertaken prior to the establishment of the TIF district to determine if the area would be "TIF eligible". Given the issues and opportunities along the corridor, the availability of TIF funding could serve as an incentive for developers. As the Village has no municipal property tax, this may not be as useful a financial tool as it is in other communities. Still, the option should be examined.

Special Service Area (SSA)

A Special Service Area (SSA) provides a means of funding improvements and programs within a designated area. In an SSA, a small percentage is added to the property tax of the properties within the defined service area. The revenue received from this targeted increase is channeled back into projects and programs benefiting those properties. An SSA can be rejected if 51% of the property owners and electors within a designated area object. SSA funded projects can include such things as district marketing and advertising assistance, promotional activities and special events, streetscape, signage improvements and property maintenance services. SSA's can also be used to fund various incentives and tools such as small business loan funds or facade improvement programs.



Business Development Districts (BDD)

A Business Development District (BDD) would allow the Village to levy up to an additional 1% retailers occupation tax, 1% hotel tax, and 1% sales tax within a designated district. Similar to a TIF district, a BDD has a maximum life of 23 years. BDD legislation also permits municipalities to utilize tax revenue growth that has been generated by BDD properties to fund improvements in the district.

- As designated in Division 74.3 of the Municipal Code of the State of Illinois, a municipality may designate, after public hearings, an area as a Business District. Business district designation also empowers a municipality to carry out a business district development or redevelopment plan through the following actions:
- Approve development and redevelopment proposals.
- Acquire, manage, convey or otherwise dispose of real and personal property acquired pursuant to the provisions of a development or redevelopment plan.
- Apply for and accept capital grants and loans from the federal and state government, for business district development and redevelopment.
- Borrow funds for the purpose of business district development and redevelopment, and issue obligation or revenue bonds, subject to applicable statutory limitations.
- Sell, lease, trade or improve such real property as may be acquired in connection with business district development and redevelopment plans.
- Business district planning activities.
- Establish by ordinance or resolution procedures for the planning, execution and implementation of business district plans.
- Create a Business District Development and Redevelopment Commission to act as an agent for the municipality for the purposes of business district development and redevelopment.

BDD funds can be used for a multitude of things including infrastructure improvements, public improvements, site acquisition, and land assemblage. Given the limited amount of funds that a BDD is capable of generating, compared to a TIF district, BDD is best suited for funding small scale improvements and property maintenance programs.

Illinois Business District Development Act

Illinois Business District Development Act Provides a source of revenue for municipalities, in the form of a 1% sales tax and a 1% hotel tax. These taxes are in addition to all existing retail occupation and service taxes and home rule sales taxes. In order to implement the tax, a municipality must determine that the proposed Business Development District is “blighted.” The definition of “blight” is similar to that used for TIF. The municipality must also determine

that, unless the Business Development District is established, the area will not experience adequate economic development. Stipulations include, a development plan that describes boundaries, estimated project costs, tax rate, and other issues. A public hearing needs to be held.

Community Development Corporations

Many communities use Tax Increment Financing (as appropriate) to fund the start up and/or operation of a Community Development Corporation (CDC) to oversee a range of redevelopment activities for a specific geographic area, particularly commercial areas and central business districts. A CDC is typically an independently chartered organization, often with not-for-profit status, that is governed by a board of directors. The directors typically bring expertise in real estate or business development along with a demonstrated commitment to the community.

CDCs are often funded through public-private partnerships with financial commitments from local financial institutions or businesses and a public funding source (TIF, etc.) to provide for both operating expenses and programs, as appropriate.

CDCs may undertake traditional chamber of commerce-like activities such as marketing, promotion, workforce development, information management, and technical assistance to small businesses, but may also administer loan programs or acquire and redevelop property in the community. Many communities create CDCs under the umbrella structure of an established chamber of commerce in the community so that missions are complementary and do not overlap.

An example of a distinctive CDC activity is the facilitation or administration of a revolving loan fund or a community lending pool capitalized by commitments from local financial institutions to provide low-interest/low-cost loans. Such funds typically target both new and expanding businesses for such redevelopment activities as interior improvements, facade and exterior improvements, building additions, site improvements, etc. Some state and federal small business assistance programs are structured to work in combination with CDC administered loan programs. Another distinctive activity of a CDC is property acquisition and redevelopment, which is most successful when the organization is mature in both expertise and capacity (particularly if the CDC intends to manage property after redevelopment).

Water & Sewer Funding Opportunities

Public Works & Development Facilities Program

This program, provided through the federal economic development administration, provides assistance to help distressed communities attract new industry, encourage business expansion, diversify local economies, and generate long-term, private sector jobs. Among the types of projects funded are water and sewer facilities, primarily serving industry and commerce; access roads to industrial parks or sites; business incubator facilities; technology infrastructure; sustainable development activities; export programs; brownfields redevelopment; and other infrastructure projects. Specific activities may

include demolition, renovation, and construction of public facilities; provision of water or sewer infrastructure; or the development of stormwater control mechanisms (e.g., a retention pond) as part of an industrial park or other eligible project.

Drinking Water State Revolving Funds (DWSRF)

The DWSRF program was established under the Safe Drinking Water Act (SDWA) Amendments of 1996 which authorizes the Agency to award capitalization grants to States, which in turn are authorized to provide low-cost loans and other types of assistance to public water systems to finance the costs of infrastructure projects needed to achieve or maintain compliance with SDWA requirements. States are also authorized to use a portion of their capitalization grants to fund a range of set-aside activities including source water protection, capacity development, and operator certification. The DWSRF program helps to ensure that the nation’s drinking water supplies remain safe and affordable and that public water systems that receive funding are properly operated and maintained.

Since the program’s inception in 1997, the States’ DWSRFs have provided \$16.2 billion in low-interest loans to public water systems and have entered into over 6,000 assistance agreements. In 2009, the DWSRF financed over 700 drinking water infrastructure projects, providing \$1.6 billion in funds. The majority of these funds were allocated to treatment, transmission and distribution projects

Clean Water State Revolving Fund

The program allows federal, state, and local agencies to leverage limited dollars. Because of the funds’ revolving nature, the federal investment can result in the construction of up to four times as many projects over a 20-year period as a onetime grant.

A major benefit for municipalities and other loan recipients is the substantial financial savings they can realize. When funded with a loan from this program, a project typically costs much less than it would if funded through the bond market. Many states offer low or no interest rate loans to small and disadvantaged communities, providing an additional incentive to getting projects started. For example, a state can make a zero-percent loan to a community for 20 years, saving the community 50 percent of the total project costs over a similar loan at 7.5 percent.

The Clean Water State Revolving Fund program’s primary mission is to promote water quality. Besides financial savings, loan recipients can realize significant environmental benefits, including protection of public health and conservation of local watersheds. Loans for such infrastructure projects also tend to stimulate local economies by encouraging commercial development and construction. CWSRFs have funded over \$89 billion, providing over 30,012 low-interest loans to date.

CWSRFs offer:

- Low interest rates, flexible terms
- Significant funding for nonpoint source pollution control and estuary protection
- Assistance to a variety of borrowers
- Partnerships with other funding sources

Key features of the program include:

- **Low Interest Rates, Flexible Term.** Nationally, interest rates for CWSRF loans average 2.2 percent, compared to market rates that average 4.5 percent. For a CWSRF program offering this rate, a CWSRF funded project would cost 19 percent less than projects funded at the market rate. CWSRFs can fund 100 percent of the project cost and provide flexible repayment terms up to 20 years.
- **Assistance to a Variety of Borrowers.** The CWSRF program has assisted a range of borrowers including municipalities, communities of all sizes, farmers, homeowners, small businesses, and nonprofit organizations.
- **Partnerships with Other Funding Sources.** CWSRFs partner with banks, nonprofits, local governments, and other federal and state agencies to provide the best water quality financing source for their communities.

Funding Mechanisms for Business & Industry

Attracting business and fostering economic development is becoming increasingly competitive. The Village of Homer Glen has potential on which to build, but to fully realize this potential, the Village must be proactive in its efforts. The actions outlined below will allow for the Village to position itself to be more competitive, but these initiatives will require upfront expenditures and an investment in resources not currently allocated.

Business Assistance & Incentives

Business Assistance Grant

This Plan acknowledges that the Village has an economic development assistance standard operating procedure in place to offer businesses in the 159th Street Corridor with development assistance.

A business assistance grant program can be utilized to attract targeted retail businesses and assist existing businesses located within a specific

area. Business assistance funds are typically offered in the form of a matching grant that pays for a certain percentage of eligible expenditures. The expenditures are typically limited to build-out costs, signage, moving expenses, and physical improvements to a property necessary to accommodate a new business or the expansion of an existing business. Priority can be given to businesses that complement the community's vision for an area and current retail needs.

The size of the grant available is typically tied to the overall impact the proposed project could have on the local area. The City of Des Plaines, for example, offers a grant of up to \$5,000 to sales tax generating uses (including food and beverage taxes) while the Village of Lombard will cover 50% of costs up to \$40,000.

Grant monies could be used to lessen the cost burden of locating or relocating to a location on the Corridor. A business assistance program would likely have the most influence in attracting small and medium-sized retail tenants and restaurants as opposed to large scale, 'big box' retailers.

Restaurant Assistance Program

A restaurant assistance program is targeted at restaurants seeking to locate within a community. As with any grant program, the Village can define exactly what types of businesses qualify for funding. For example, the assistance program could be limited to full-service restaurants only by specifying a minimum threshold

for the percent of gross income derived from sales made for on-site consumption.

The funds are intended to assist with renovating existing space to accommodate restaurant use or in the construction of a new restaurant and are typically limited to improvements to real property (i.e. kitchen equipment is ineligible). Funds are typically offered as a matching grant with a requirement that the business remain open for a minimum number of years. A lien is placed on the subject property which is then reduced by a proportionate amount for every year of operation and forgiven after completion of minimum period of operation.

The Village of Lisle has a restaurant loan program targeted at lessening the costs of renovating and retrofitting existing commercial space for restaurant use or demolishing an existing building for the construction of a new restaurant. Lisle will match funds up to 50% of the total project costs with a grant ceiling of \$200,000. A lien for the grant amount is recorded on the property for a five year period after which the lien is released.

Commercial Loan/"Gap" Financing Program

Securing funding for development can be a long and complicated process and it has become increasingly common for projects to rely on multiple funding sources to come to fruition. Since the economic downturn, conventional lenders have reduced the

amount of risk they are willing to assume with a given project. Municipalities, counties and other public economic development entities have played an increasing role in providing "gap" financing to make up for shortfalls in financing provided by other financial institutions.

Commercial loan/"gap" financing programs can be established to lessen the risk to conventional lenders while providing more favorable loan terms to businesses looking to relocate or expand locally. Such loan programs typically stipulate that the program cannot be the primary lender of the project. Loan terms may range, but are typically either a short term loan to cover immediate capital needs before sufficient long term financing can be secured, or a secondary loan that coincides with the life of the primary loan. Regardless, the goal is to encourage and facilitate development by providing a project with a blended financial package that is below market rate.

As an example, the Berwyn Development Corporation and the City of Berwyn offer a commercial loan program targeted at providing incentives to renovate or expand existing commercial space or to new businesses looking to locate in the community. The program is capitalized by money from the municipality and lines of credit extended to partner financial institutions and guaranteed by the City. Loan terms vary depending on the goals of the project (e.g. startup

capital, job creation, business modernization, etc.) and exist in two tiers comprising loans with a principal amount above or below \$40,000.

Sales Tax Rebate

A sales tax rebate is a tool typically used by municipalities to incentivize businesses to locate to a site or area. The rebate is offered as a percentage of the annual sales tax revenue generated by the establishment and is often tied to benchmarks such that as sales volume increases, so too does the proportion of the rebate. Sales tax rebate percentages can range from 1% to 100% and are dependent on the goals and objectives of the local municipality. Sales tax rebates have proven effective in attracting new businesses and encouraging redevelopment and renovation. Home rule communities such as Homer Glen also have the authority to issue bonds based on sales tax revenue. As stated previously, it is understood that the Village does have a policy in place to assist businesses seeking sales tax rebates.



Site Improvement Grant

While new development must adhere to the Village's guidelines, a site improvement grant could be utilized to revitalize existing development and ensure complementary site design and amenities between new and old.

The Downers Grove Economic Development Corporation (EDC) offers a grant program for properties located along one of its prominent commercial corridors, Ogden Avenue. Property owners are reimbursed for up to 50% of project costs related to improving the appearance of the property or building, providing updated signage, and improving access and circulation for both pedestrians and automobiles. The grant is funded through a TIF District in place along the corridor and the grant ceiling is either \$50,000 or \$75,000 depending on the location of the subject property.

Facade Improvement Program

The purpose of a Facade Improvement Program is to encourage projects which contribute to the economic revitalization and character of an area by providing financial and technical assistance for facade improvements. Building facades, both individually and collectively, create a strong first impression of an area. Redevelopment is not the only opportunity to establish an architectural tone for the 159th Street Corridor. By implementing a Facade Improvement Program,

current property owners are provided an opportunity to improve their outdated or failing structures without having to relocate.

The Village currently has a Facade Improvement Loan Program in place. Any commercial (retail and/or service) buildings can qualify for the B-FIP program with no minimum or maximum value of privately funded facade improvement. The Village will loan up to 50% of the cost of approved facade improvements, up to a maximum of \$7,500.00. The B-FIP loans are being granted at 0% interest, utilizing the Village of Homer Glen Business Revolving Loan Fund.

Business Incentive Non-Conforming Sign Replacement Loan Program

Commercial business improvements can help create visually attractive shopping districts that encourage further visitation and investment for Homer Glen. To assist business with their appearance, the Village offers a Non-Conforming Sign Replacement Loan Program. The program offers financial assistance to any commercial building/business in Homer Glen that currently has a non-conforming monument or wall sign. The Village will loan up to 50% of the cost of new signage, to a maximum of \$5,000.00, with 0% interest, using the Village of Homer Glen Business Revolving Loan Fund. The loan must be utilized to bring existing non-conforming signage into compliance with the Village's sign code.

Light Industrial Development

There are also several county, state, and federal incentive programs that the Village can utilize to attract industrial users to the area or expand existing employers along the Corridor.

Industrial Revenue Bond (IRB) Inducements

Industrial Revenue Bonds (IRBs) offer a tax-exempt, low interest source of financing to manufacturing firms. A municipality may sell IRBs and then loan the proceeds to manufacturing businesses to finance capital investment projects. IRBs are unique in that although the municipality issues the bonds, the firm benefiting from the capital investment is to provide the interest and principal payments on the loan. In effect, the municipality is lending its bond tax-exemption, but not its credit, to the bond issue. Municipalities typically place limits on the minimum and maximum size of the bond issue and the maximum size of the project being funded. IRBs offer long-term financing rates lower than conventional financing, fixed or variable rate financing, and financing for up to 100% of a project's costs.

Illinois Economic Development for a Growing Economy (EDGE) Tax Credit Program

The Illinois Department of Commerce and Economic Opportunity's EDGE program offers tax incentives to encourage companies to locate or expand operations in Illinois as opposed to locations a company may be considering in other states. Tax credits may be provided in amounts equal to the state income taxes withheld from employees occupying new positions created by the project. The non-refundable credits can be used against corporate income taxes to be paid over a period not to exceed 10 years.

Companies applying to the EDGE program must document their active consideration of a competing location in another state. The project must include at least \$5 million in capital improvements and create a minimum of 25 full-time jobs. For a company with 100 or fewer employees, the project must include at least \$1 million in capital improvements and create at least 5 full-time jobs.

Large Business Development Program

Similar to the EDGE program, the Illinois Department of Commerce and Economic Opportunity's Large Business Development Program (LBDP) provides incentive financing to encourage large out-of-state companies to

locate in Illinois. It also provides incentives for existing large companies to undertake substantial job expansion or retention projects.

Funds available through the program can be used by businesses with 500 or more employees for typical business activities, including financing the purchase of land and buildings, construction or renovation of fixed assets, and site preparation. LBDP funds are targeted to extraordinary economic development opportunities, that is, projects that will result in substantial private investment and the creation and/or retention of 300 or more jobs. Grant eligibility and amounts are determined by the amount of investment and job creation or retention involved.

Participation Loan Program

The Illinois Department of Commerce and Economic Opportunity's Participation Loan Program (PLP) is designed to work through banks and other conventional lending institutions to provide subordinated financial assistance to Illinois small businesses that employ Illinois workers. A business with 500 or fewer employees may apply for a PLP loan of between \$10,000 and \$750,000 that is not to exceed 25% of the total project.

Eligible project costs that funds can be applied to include the purchase and installation of machinery and equipment, working capital, the purchase of land, and the construction or renovation of buildings.

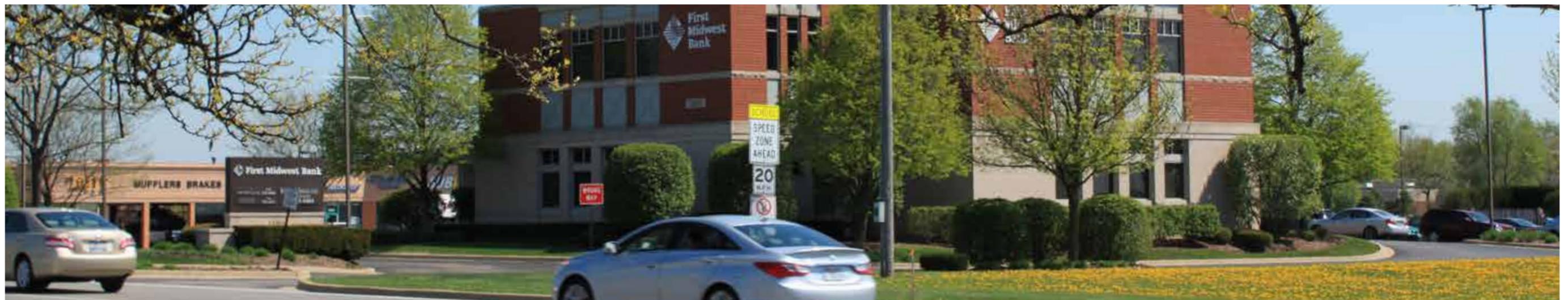
PLP funds may not be used for debt refinancing or contingency funding. Lending institutions are responsible for reviewing applications for eligibility and setting loan terms.

Development Matrix

The following table has been prepared to assist the Village in assessing development opportunities along the 159th Street Corridor. The table assesses the basic building and site requirements for a variety of uses and development types. While this table has been prepared for the 159th Street Corridor, it can also be applied to other locations within the Village.

Major Action Item Matrix

The Major Action Item Matrix offers a list of all implementation strategies, key policies and recommendations found in the previous sections of the 159th Street Corridor Planning Study. From this matrix, Village staff will be able to prioritize implementation activities to be completed over the Plan's lifespan. The Major Action Item Matrix is presented in a table format which offers a brief description of each action/project and identifies the priority level and lead responsibility.



Development Matrix

Use	Examples	Typical Size (square feet)	Typical Height (stories)	Does it require exposure?	Are traffic counts important?	Infill Locations?
Retail						
Grocery Store	Jewel, Domincks, Marianos, Whole Foods	35,000 to 70,000	1	Yes	Typically, but decent visibility and access from high traffic area okay.	Typically not, unless larger urban environment
Specialty Grocer	Trader Joes	10,000 to 20,000	1	Yes	Typically, but decent visibility and access from high traffic area okay.	Typically not, unless larger urban environment
Convenience Store	7-11, KWIK Mart	2,500 to 7,500	1	Yes	Yes	Yes
Pharmacy	CVS, Walgreens	5,000 to 15,000	1	Yes	Typically, but decent visibility and access from high traffic area okay.	Typically not, unless larger urban environment
Gift Shop	Hallmark, local	1,500 to 5,000	1	Yes	Pedestrian orientated environment can overcome low traffic counts	Yes
Big Box						
Big Box Retail	HH Greg, Target, Wal-Mart, Petco	50,000+	1	Yes	Typically, but decent visibility and access from high traffic area okay.	No
Warehouse Clubs	Costco, Sams Club	100,000+	1	Yes	Typically, but decent visibility and access from high traffic area okay.	No
Electronics Smaller Scale	New Best Buy format, Game Stop, Apple	1,500 to 15,000	1	Yes	Pedestrian orientated environment can overcome low traffic counts	Yes
Apparel	J Crew, Guess, Anthropologie	5,000 to 12,000		Yes	Pedestrian orientated environment can overcome low traffic counts	Yes
Department Stores (free standing)	Kohl's, Sears	60,000 to 100,000	2	Yes	Typically, but decent visibility and access from high traffic area okay.	No
Home Improvement	Lowe's, Home Depot	100,000 to 150,000	1	Yes	Typically, but decent visibility and access from high traffic area okay.	No
Hardware (small)	Do it Best	5,000 to 10,000	1	Yes	Yes	Yes
Restaurants						
Full Service (Local)	White table cloth type		1 to 2	(depends on location and following)	Depends on use. Pedestrian orientated environment can overcome low traffic counts	Yes
Full Service (Chain)	Chilis, BW3, Olive Garden	2,500 to 5,000	1	Yes	Typically, but decent visibility and access from high traffic area okay.	No
Limited Service	Panera, Chipotle, Starbucks	1,500 to 3,000	1	Yes	Typically, but decent visibility and access from high traffic area okay.	Yes
Fast Food	McDonalds, Burger King	1,500 to 3,000	1	Yes	Yes	Typically not, unless larger urban environment
Beer, Wine, Liquor	Binnys, Local	2,500 to 7,500	1	Yes	Depends on use. Pedestrian orientated environment can overcome low traffic counts	Yes
Office Supply	Staples, Office Max	10,000 to 25,000				Yes

Use	Examples	Typical Size (square feet)	Typical Height (stories)	Does it require exposure?	Are traffic counts important?	Infill Locations?
Entertainment/Recreation						
Theater (multi screen)	AMC or similar	Dependent on number of screens	2-3	Yes	Typically, but decent visibility and access from high traffic area okay.	No
Health Club	XSport, LA Fitness, Independent	10,000 to 100,000	1-4	Yes	Typically, but decent visibility and access from high traffic area okay.	Yes
Entertainment Center	Brunswick Zone, Lucky Strike	20,000 to 100,000	1-3	Yes	Typically, but decent visibility and access from high traffic area okay.	Typically not, unless larger urban environment
Hotel						
Limited Service	Hilton Garden, Hampton Inn	Depends on property	3 to 5	Yes	Typically, but decent visibility and access from high traffic area okay.	Typically not, unless larger urban environment
Full Service	Marriott, Hyatt Regency		5+	Yes	Typically, but decent visibility and access from high traffic area okay.	Typically not, unless larger urban environment
Boutique Hotel	Usually not a chain	Depends on property	3-5	No	Depends on property	Yes
Mixed Use						
Office above retail		varies by development	2+	Yes	Depends on use. Pedestrian orientated environment can overcome low traffic counts	Yes
Residential above retail		varies by development	2+	Yes	Depends on use. Pedestrian orientated environment can overcome low traffic counts	Yes
Office Alone						
Professional Space	Financial, medical,	varies by use	1+	No	No	Yes
Corporate	Headquarter property	varies by use	1+	No	No	No
Residential Alone						
Condominiums	For Sale multi-family attached units	varies by development	2+	No	No	Yes
Rowhomes/Townhomes	For Sale single-family attached units	varies by development	2-3	No	No	Yes
Apartments	Rental multi-family attached units	varies by development	2+	No	No	Yes

Major Action Item Matrix

	Priority	Lead Responsibility
Chapter 3 – Land Use & Development Plan		
Work with property owners and developers to ensure that water and sewer infrastructure is installed in order to make 159th Street available for optimal development.	1	Village Board
Coordinate with local property owners and developers to ensure that all new, improved, and existing commercial activity and development is effectively screened and buffered from adjacent residential uses by including additional landscaping requirements and appropriate set backs.	1	Planning and Zoning
Establish communication with local property owners and developers to ensure that all commercial developments provide for safe, attractive, and efficient access and circulation of pedestrian and bicycle activity.	1	Planning and Zoning
Establish communication with institutions and developers to ensure that existing institutions within the Corridor are not adversely affected by new development and vice versa.	1	Planning and Zoning
Continue to update the Zoning Ordinance and Zoning Map to reflect the desired development for the existing agricultural parcels along 159th Street.	1	Planning and Zoning
Coordinate with local property owners and developers to ensure that the parcels on the west end of the Corridor, most proximate to I-355, cater to large scale retail centers, hotel, light-industrial, and/or business/office parks.	1	Economic Development
Establish communication with local property owners and developers to ensure the use of townhome and duplex residential areas within the Corridor and create a transition between single family uses and non-residential uses, while at the same time providing greater housing options in the community.	2	Planning and Zoning, Economic Development, Village Board
Retain and enhance existing multi-family developments within the Corridor.	2	Village Board
Ensure newly designated office sites should remain flexible, as they may also be capable of accommodating a range of non-retail uses as well.	3	Planning and Zoning, Economic Development

	Priority	Lead Responsibility
Chapter 4 – Design & Development Guidelines		
Ensure proper application of the Design and Development guidelines by property owners, developers, and the Village in order to accommodate development that improves the Corridor's aesthetics and reinforces and enhances the existing and desired character for the Village.	1	Planning and Zoning
Ensure that the Design and Development Guidelines are used to review plans and proposals for all new development within the community.	1	Planning and Zoning

	Priority	Lead Responsibility
Chapter 5 – Streetscape & Beautification Plan		
Work with property owners and developers to ensure that water and sewer infrastructure is installed in order to make 159th Street available for optimal development.	1	Village Board
Install sidewalks to connect adjacent commercial and residential neighborhoods to the Corridor. In some cases, the Village will need to work with private property owners to obtain the land needed to fill sidewalk gaps.	1	Development Services
Make improvements at key intersections along 159th Street.	1	Village Board, Highway Department
Work with property owners to acquire necessary easements to install new village gateway signs in the areas recommended on the Streetscape & Beautification Plan.	1	Planning and Zoning, Development Services
Develop and implement a Streetscape Plan based on the recommendations in this plan.	1	Planning and Zoning
Consider new Landscape Guidelines to convey a consistent landscape appearance along the Corridor. Landscape Guidelines should provide a phasing strategy to ensure that landscaping is installed after development construction.	1	Planning and Zoning, Development Services
Provide regular general maintenance and upkeep of “fringe” areas between the parking lot of a business and 159th Street.	1	Highway Department
Work with local property owners and developers to ensure that pedestrian scale improvements work in conjunction with the multi-use path and sidewalk to create a welcoming pedestrian development.	2	Planning and Zoning
Consider the pedestrian and bicycle access when establishing future development, while still accommodating automobile traffic.	2	Planning and Zoning,
Install a comprehensive wayfinding signage system to guide motorists and pedestrians to key locations within the Village such as parks, open spaces, event areas, Village Hall, and other municipal facilities.	2	Development Services, Highway Department
Coordinate efforts with residents and businesses along the 159th Street Corridor to maintain a consistent line of street trees, where it is feasible.	2	Planning and Zoning

Major Action Item Matrix

	Priority	Lead Responsibility
Chapter 6 – Access & Transportation Plan		
Alleviate concerns, facilitate a smooth transition, and offer assistance by offering businesses "We're Open" signage during roadway construction.	1	Economic Development
Limit access from 159th Street by creating alternative access through public side streets, service roads, or internal connections with neighboring developments, that may help to increase accessibility on the median separated roads.	1	Highway Department, IDOT, Village Board
Identify existing curb breaks to internal connectors and create new internal connectors to allow multiple businesses to benefit from one single curb access point or break in the median.	1	Planning and Zoning, Development Services
Emphasize full access from side streets and consolidated entrances, spaced far enough apart, to allow for dedicated turn lanes.	1	Planning and Zoning, Highway Department, IDOT
Communicate with businesses to encourage to reconfigure parking lots to allow for cohesive cross access drives to connect properties.	1	Planning and Zoning, Economic Development
Implement an On-Site Improvement Program to assist current property owners in upgrading their existing parking lots and installing on-site landscaping.	1	Planning and Zoning, Economic Development
Adopt amortization requirements as it relates to properties with non-conforming parking lots to encourage business owners to bring their properties up to code over an acceptable period of time.	1	Village Board
Promote the availability and use of Pace on the Corridor as a means of accessing commercial businesses, producing increased access to retailers, as well as providing transit to and from the nearby Metra stations.	1	Economic Development
Coordinate with Pace to designate Pace bus stops (including, shelters, information kiosks, etc.) as part of future streetscape improvements programs implemented within the Corridor.	1	Economic Development
Create connections to the IDOT multi-use path by filling in gaps where there are no sidewalks, replacing surfaces that are damaged or unsafe, and ensuring that all segments meet accessibility requirements related to width, obstructions, etc.	1	Development Services
Limit existing and potential excessive parking along 159th Street due to high traffic volumes and the type of development it will procure.	2	Planning and Zoning
Coordinate with local businesses to ensure that secure, highly visible, and convenient bike parking is available.	2	Planning and Zoning
Establish bike paths that lead to shopping developments.	2	Development Services
Coordinate with property owners, utility companies, and developers should be established in order to bury the utility lines or relocate them to the rear of future developments.	2	Development Services, Economic Development
Promote the Village's existing incentive program for updating commercial roadway signage.	3	Economic Development
Communicate with businesses to encourage the removal of pedestrian obstructions such as fences and walls to allow for pedestrian connections between properties.	3	Economic Development
Consider encouraging native plantings, bioswales, and pervious pavers to improve stormwater management within commercial and off-street parking areas	3	Planning and Zoning, Development Services
Coordination efforts with the Forest Preserve District to provide trailhead bike parking for travelers wishing to visit business and other destinations along the 159th Street Corridor.	3	Village Board

	Priority	Lead Responsibility
Chapter 7 – Infrastructure & Utilities Plan		
Require developers and utility companies to screen utility boxes from the street either through landscaping, fencing, or both.	1	Village Board
Coordination with utility providers and jurisdictional agencies to establish carefully placed utility boxes, consolidate boxes where possible, and standardize materials and color in order to minimize the visual impact of the utility boxes, whenever utilities are modified or upgraded.	1	Development Services
Develop a program that identifies priority locations for water and sewer infrastructure extension along the Corridor to ensure thoughtful expansion of utilities.	1	Village Board
Utilize the Water Resource Management Ordinance and Water Resources BMP Guidelines when reviewing development proposals for the 159th Street Corridor.	1	Development Services
Parallel IDOT stormwater improvements along the Corridor with improvements along new and existing entranceways and side streets.	1	Development Services
Encourage stormwater management on private property through Village-sponsored financial incentives (i.e. matching funds, grants, low interest loans, permit rebates, etc.) to assist local property owners interested in making on-site stormwater improvements to their properties.	1	Village Board
Minimize the number of stormwater basins along the Corridor to maximize developable space.	1	IDOT, Village Board